



REGULAR BOARD MEETING

Wednesday, August 3, 2016, 2:00 pm

AGENDA

1. Call to Order
2. Pledge of Allegiance
3. Roll Call
4. Approval of Minutes
 - *June 1, 2016 Regular Board Meeting*
5. Comments from the Public
6. Comments from Advisory Committee Members (TAC/CAC/BPAC)
7. Approval of Agenda
8. Consent Agenda
 - 8a. Amendment to the FY 2016/17 – FY 2020/21 Transportation Improvement Program (TIP): Notification of an administrative amendment to the TIP to incorporate the FY 2016/17 Roll-Forward Report.

Action: Approve or disapprove the notification.
9. Action Items
 - 9a. **Executive Director's Performance Review:** Review of the Executive Director's performance for FY 2015/16.

Action: Approve the Executive Director's performance review, approve with conditions, or do not approve.
 - 9b. Staff Performance Bonus Program: Consideration of the 2016 Performance Bonus Program for TPO staff.

Action: Approve the 2016 Staff Performance Bonus Program, approve with conditions, or do not approve.

- 9c. 2016/17 List of Priority Projects (LOPP): Review and adoption of the 2016/17 LOPP for the St. Lucie TPO.

Action: Adopt the LOPP, adopt with conditions, or do not adopt.

- 9d. Walton Road Multimodal Improvements Feasibility Study: Review and acceptance of the revised Walton Road Multimodal Improvements Feasibility Study.

Action: Accept the study, accept with conditions, or do not accept.

- 9e. Public Involvement Program (PIP) Annual Evaluation of Effectiveness and Update: Review of the effectiveness of the St. Lucie TPO's PIP during FY 2015/16.

Action: Accept the PIP Annual Evaluation of Effectiveness and Update, accept with conditions, or do not accept.

10. FDOT Comments

11. Recommendations/Comments by Members

12. TPO Staff Comments

13. Next Meeting: The next St. Lucie TPO Board Meeting is a regular meeting scheduled for 2:00 pm on Wednesday, October 5, 2016.

14. Adjourn

NOTICES

The St. Lucie TPO satisfies the requirements of various nondiscrimination laws and regulations including Title VI of the Civil Rights Act of 1964. Public participation is welcome without regard to race, color, national origin, age, sex, religion, disability, income, or family status. Persons wishing to express their concerns about nondiscrimination should contact Marceia Lathou, the Title VI/ADA Coordinator of the St. Lucie TPO, at 772-462-1593 or via email at lathoum@stlucieco.org.

Persons who require special accommodations under the Americans with Disabilities Act (ADA) or persons who require translation services (free of charge) should contact Marceia Lathou at 772-462-1593 at least five days prior to the meeting. Persons who are hearing or speech impaired may use the Florida Relay System by dialing 711.

Items not included on the agenda may also be heard in consideration of the best interests of the **public's health, safety, welfare, and as necessary to protect every person's right of access**. If any person decides to appeal any decision made by the St. Lucie TPO with respect to any matter considered at this meeting, that person shall need a record of the proceedings, and for such a purpose, that person may need to ensure that a verbatim record of the proceedings is made which includes the testimony and evidence upon which the appeal is to be based.

Kreyòl Ayisyen: Si ou ta renmen resewwa enfòmasyon sa a nan lang Kreyòl Ayisyen, tanpri rele nimewo 772-462-1593.

Español: Si usted desea recibir esta información en español, por favor llame al 772-462-1593.

REGULAR BOARD MEETING

DATE: Wednesday, June 1, 2016
 TIME: 2:00 pm
 LOCATION: St. Lucie TPO
 CoCo Vista Centre
 466 SW Port St. Lucie Boulevard, Suite 111
 Port St. Lucie, Florida

MINUTES

1. Call to Order

Chairwoman Hensley called the June 1, 2016 regular meeting of the St. Lucie Transportation Planning Organization (TPO) Board to order at 2:01 pm.

2. Pledge of Allegiance

Chairwoman Hensley led the Pledge of Allegiance.

3. Roll Call

The roll was called. A Quorum was noted with 11 members in attendance.

Members present:

Kathryn Hensley, Chairwoman
 Mayor Linda Hudson, Vice Chair
 Councilwoman Michelle Berger
 Darrell Drummond
 Commissioner Chris Dzadoovsky
 Commissioner Frannie Hutchinson
 Commissioner Paula Lewis
 Councilwoman Shannon Martin
 Commissioner Tod Mowery

Representing:

St. Lucie County School Board
 City of Fort Pierce
 City of Port St. Lucie
 Community Transit
 St. Lucie County
 St. Lucie County
 St. Lucie County
 City of Port St. Lucie
 St. Lucie County

Mayor Greg Oravec
Commissioner Reginald Sessions

City of Port St. Lucie
City of Fort Pierce

Others attending:

Peter Buchwald
Ed DeFini
Yi Ding
Marceia Lathou
Mary Holleran
Crystal Wilson
Jack Andrews
William T. Evans
Linda Ferreira
Lois Bush
Myroslava Skoroden
Roxanne Chesser
Heather Young, Esq.
Alice Bojanowski

Representing:

St. Lucie TPO
St. Lucie TPO
St. Lucie TPO
St. Lucie TPO
Recording Specialist
Placevision, Inc.
City of Fort Pierce
Stanley Consultants
Stanley Consultants
FDOT District 4
FDOT District 4
City of Port St. Lucie
St. Lucie County Attorney
Martin County MPO

- 4. Approval of Minutes
 - April 6, 2016 Regular Meeting

* MOTION – MOVED by Commissioner Dzadovsky to approve the Minutes of the April 6, 2016 Regular Board Meeting.

* SECONDED by Commissioner Lewis Carried UNANIMOUSLY

- 5. Comments from the Public – None

- 6. Comments from Advisory Committee Members (TAC/CAC/BPAC) - None

- 7. Approval of Agenda

* MOTION – MOVED by Mr. Drummond to approve the Agenda.

** SECONDED by Councilwoman Martin Carried UNANIMOUSLY

- 8. Consent Agenda

8a. Transportation Disadvantaged (TD) Planning Grant Application: Adopt or do not adopt.

* MOTION - MOVED by Mayor Hudson to approve the Consent Agenda.

** SECONDED by Commissioner Lewis Carried UNANIMOUSLY

9. Action Items

9a. FY 2016/17 – FY 2020/21 Transportation Improvement Program (TIP): Review and adoption of the draft FY 2016/17 – FY 2020/21 TIP for the St. Lucie TPO.

Mr. Buchwald explained the purpose of the TIP is to identify prioritized transportation improvement projects in the TPO area receiving state funding over a five year period. The Final Tentative Work Program which is a primary component of the TIP was received from FDOT in April 2016.

Mr. Yi Ding, TIP Program Manager, presented highlights and a summary of the draft FY 2016/17 – FY 2020/21 TIP. The year-long development process of the TIP will begin in June 2016.

Board Comments and Questions:

Councilwoman Martin addressed Mowing and Litter, C-4-3, C-4-4 and asked where it came from. Mr. Buchwald explained this was a general contract from FDOT for routine maintenance of the State Highway system through the St. Lucie area.

Mayor Oravec questioned the budgeting process going through the TPO for this maintenance. Mr. Buchwald explained that areas had to be identified for approval for state funding through the TPO Work Program process.

Mayor Hudson addressed Operation and Maintenance of Traffic Control Systems (C 3-5) with money being spent for Fort Pierce in the 5th year and asked how that was allotted every year for the City of Port St. Lucie and the County.

Mr. Buchwald explained that occurs through a joint agreement with FDOT and each jurisdiction for that operation throughout the state system.

Mayor Hudson questioned what was entailed with repairs to the South Bridge. Mr. Buchwald said it included maintenance of the Bridge only, and nothing else.

Commissioner Lewis addressed C-4-2, funding for operational improvements on the rail capacity project, noting that Amtrack has been out of consideration for some time. Mr. Buchwald explained that project has been funded to extend Amtrack service on the rail line for the past five years, and the money is just sitting there waiting for a state decision.

Councilwoman Martin commented that if money is unused through the TIP that staff look at look at moving up some of the Paar Drive projects to keep them in line with those projects that are under construction.

Chairwoman Hensley indicated that would be a project for staff to look at for next year's Plan which begins next week.

Councilwoman Berger agreed with moving up complementary construction projects in the Port St. Lucie Boulevard, Becker Road area with those in the Plan. It would benefit jobs in that area, having one complete project versus construction of just one phase.

Commissioner Dzadvosky commented on the concern for repair to the Citrus Avenue Bridge with the possibility of a passenger train going over the tracks.

* MOTION – MOVED by Councilwoman Berger to adopt the draft FY 2016/17 – FY 2020/21 TIP for the St. Lucie TPO.

** SECONDED by Mayor Hudson Carried UNANIMOUSLY

9b. Transportation Alternatives Program (TAP) 2016 Grant Application: Endorse the TAP grant application for the 2016 grant cycle, endorse with conditions, or do not endorse.

Mr. Buchwald explained the purpose and the amount of funding for the TAP 2016 Grant Application. Only one application was submitted, from the City of Port St. Lucie for construction of a five foot wide, one mile long concrete sidewalk on the south side of North Macedo Boulevard between St. James Drive and Selvitz Road.

This project is ranked 7th on the TA Priority Project list and is recommended for endorsement by the Board.

DRAFT

Commissioner Hutchinson asked why the 7th ranked project was selected before the 1st ranked project. Mr. Buchwald explained that project was the only application received for the grant. Discussion ensued on sidewalks being a major concern.

MOTION – MOVED by Councilwoman Berger to endorse the TAP 2016 Grant application presented for the allocation of TAP funding from the 2016 grant cycle.

SECONDED by Councilwoman Martin Carried UNANIMOUSLY

Commissioner Dzadvosky commented on being in discussions about the surtax and with the possibility of lifting items, to look at grants for funding opportunities.

10. Discussion Items

10a. Walton Road Multimodal Improvements Feasibility Study
No Action. Discuss and provide comments.

Mr. Edward DeFini, TPO Bicycle-Pedestrian Program Manager introduced Ms. Linda Ferreira, Stanley Consultants, Inc., to review the Walton Road Multimodal Improvements Feasibility Study.

Ms. Ferreira provided a PowerPoint presentation concentrating on connectivity and safety on and off the road. Study Alternatives with concepts for future designs were presented in three segments – A, B, and C, and kayak crossing options either under or over Walton Road were evaluated. Cost estimations were provided.

TPO Board Comments and Questions (summarized):

Did costs include the time needed for getting permits going through the Savannas? The costs that were included were explained.

On the elevated multimodal section were ramps included for pedestrians and bicycles. Alternative "C-3" showed steps for the boardwalk, which was a ramp.

The cost and design of kayak crossings going under and over the road was discussed.

Segment "B" relating to the south side is easier and would tie-in to the section the County has already done on Walton Road to U.S. #1.

DRAFT

Comments about the worth of building bike lanes that are not buffered from traffic, the safety hazards and lack of shoulders and questions about who really uses them, were brought forth.

Mayor Oravec thought Segment "C" would be difficult, he supported arched culverts and discussed maintenance, life span, benefits and costs. The annual costs have to be factored in to make a decision. Sidewalks placed directly next to asphalt and curbs are not desirable.

Alternative C-1 and C-2 were discussed along with the amount of right-of-way needed to improve the corridor, curbs, planting areas for protection and the ability to go under the boardwalk.

The boardwalk fix might be a temporary plan. Permitting may be easier and improve the connectivity of the water bodies. A boardwalk allows for people to gather when walking and biking.

The anticipated time line for the Study to go before the Advisory Committees is July, and the Study is anticipated to return to the Board for acceptance at the August meeting.

10b. Strategic Intermodal System (SIS) Multimodal Needs Plan Update: No action. Discuss and provide comments.

Mr. Buchwald introduced Ms. Lois Bush, FDOT District 4, to share and update information on the Strategic Intermodal System (SIS) Multimodal Needs Plan.

Ms. Bush provided an in-depth review of Florida's statewide network of high priority transportation facilities, including airports, railroads, highways, seaports, spaceports and transit facilities.

The SIS identifies capacity projects needed to meet Florida Transportation Plan Goals and SIS Policy Plan Objectives. FDOT's process to update the SIS Needs Plan from 2040 to 2045, which began in 2016 and is scheduled to conclude in March 2017.

Projects associated with SIS facilities are eligible for funding dedicated to the SIS, and must meet the designated criteria and thresholds identified in the Adopted Summary of SIS Facility Types, Criteria and Thresholds.

TPO Board Comments and Questions:

Commissioner Dzdvosky did not see any information relating to the Airport as an emerging system to support a connector to the Airport, the Port, rail, I-95, and the turnpike, which he believed should be part of this funding. He commented on the growth and commercial manufacturing being developed in a trade zone to support and encourage funding for the Airport.

Ms. Bush explained the designated SIS criteria and said funding is objectively based on activity criteria provided in their Long Range Plans. Projects potentially eligible for SIS funding have to be identified for inclusion in the update to the FDOT SIS Multimodal Needs Plan currently underway. She commented on development of a long range master plan that includes St. Lucie, Martin and Indian River Counties as an emerging hub with connectors.

Mr. Buchwald explained that FDOT will be back at the conclusion of the Needs process to guide and provide assistance with identifying projects to be included in the Needs Plan. He noted the importance of how the potential SIS facilities are packaged and the characteristic factors. Staff and FDOT will work together and carefully review the potential SIS connectors that are inadequate to handle freight traffic that should be re-designated and moved.

Commissioner Mowery asked if this was an item to create a request to the District for a designation change. Ms. Bush explained the process for the designation of a hub connector and said requests for changes are appropriate.

Commissioner Dzdvosky commented on getting interchanges at I-95 and the Turnpike funded through a developer and getting support from the Turnpike Commission bond money. Mr. Buchwald indicated the key is how that project was identified as a connector to the airport.

Mayor Oravec asked if any of our Plans have articulated our need, that we want direct roadway access to connect the Turnpike to I-95, to the Airport to the Port. It is a top five issue for St. Lucie County.

Mr. Buchwald noted the Long Range Plan identifies the potential connection between the Turnpike, I-95, the Port and the Airport and the SIS highway connector is inadequate for such use. The purpose of the Needs Plan is to identify to FDOT unfunded Multimodal transportation needs potentially eligible for SIS funding.

Ms. Bush added the SIS connector is based on current traffic patterns and existing facilities, and can evolve over the long term. The

importance of the planning process includes eligibility criteria, the facilities purpose and all plans are connected through the Needs and Cost Feasible 5-year plans.

Drafts will be prepared in October and will come to this Board before it is adopted by the FDOT Executive Board in March 2017.

Chairwoman Hensley advised the Board to have their Needs Plan update information in to Mr. Buchwald or to Ms. Dykstra/Ms. Bush at FDOT District 4.

11. FDOT Comments - Mr. Buchwald introduced FDOT District 4 new Liaison Myroslava Skoroden. Ms. Skoroden, Planning Specialist, said she will be attending TPO meetings on a regular basis and was happy to here.
12. Recommendations/Comments by Members – None
13. TPO Staff Comments – Mr. Buchwald said they are continuing to promote the process to develop the list of priority projects for the LOPP. The District will provide estimates of revenue funding and to look at how it can be allocated among the priority projects and advancing projects. Local staffs are asked to review these lists, update the project information and internally discuss and consider any changes, additions or revisions as it continues to the Long Range Plan. Please consider any changes in local funding that has occurred with your budgeting process.

After the Advisory Committees review the list and consider their recommendations in July, the list will come before the TPO Board for their consideration and adoption at the August 3rd meeting. Members were wished a happy and safe summer.
14. Next Meeting: The next meeting is a regular TPO Board meeting scheduled for 2:00 pm on Wednesday, August 3, 2016.
15. Adjourn – The meeting was adjourned at 3:35 pm.

Respectfully submitted:

Approved by:

DRAFT

Mary F. Holleran
Recording Specialist

Kathryn Hensley
Chairwoman

AGENDA ITEM SUMMARY

Board/Committee:	St. Lucie TPO Board
Meeting Date:	August 3, 2016
Item Number:	8a
Item Title:	Amendment to the FY 2016/17 – FY 2020/21 Transportation Improvement Program (TIP)
Item Origination:	Florida Department of Transportation (FDOT) District 4 Work Program
UPWP Reference:	Task 3.3 – TIP
Requested Action:	Approve or disapprove the notification of an administrative amendment to the TIP to incorporate the FY 2016/17 Roll Forward Report which contains the projects in the previous TIP that were not authorized by June 30, 2016.
Staff Recommendation:	Based on the TIP amendment being developed and approved in compliance with applicable policies and procedures, it is recommended that the notification be approved.

Attachments

- TIP Amendment #1 Approval Certification Form
- FY 2016/17 Roll-Forward Report



TIP Amendment Number 1

FY 2016/17 – FY 2020/21 TRANSPORTATION IMPROVEMENT PROGRAM
 ADMINISTRATIVE AMENDMENT APPROVAL CERTIFICATION FORM

FY 2016/17 Roll-Forward Report

The St. Lucie Transportation Planning Organization (St. Lucie TPO), through administrative delegation to its Executive Director, approved incorporation of the attached report into the FY 2016/17 – FY 2020/21 Transportation Improvement Program (TIP) adopted on June 1, 2016. This amendment will be recognized by the Federal Highway Administration after the TIP becomes effective with regard to the start of the Federal fiscal year on October 1, 2016.

I attest that this TIP administrative amendment was developed and approved in compliance with applicable policies and procedures.

Peter Buchwald

 Executive Director
 St. Lucie TPO

7/26/16

 Date

FLORIDA DEPARTMENT OF TRANSPORTATION
 OFFICE OF WORK PROGRAM
 MPO ROLLFORWARD REPORT
 =====
HIGHWAYS
 =====

ST. LUCIE TPO

ITEM NUMBER:431752 2 PROJECT DESCRIPTION:PORT ST. LUCIE BLVD FROM PAAR DRIVE TO DARWIN BLVD *NON-SIS*
 DISTRICT:04 COUNTY:ST. LUCIE TYPE OF WORK:ADD LANES & RECONSTRUCT
 EX DESC:2015 TPO #1 WIDENING FROM 2 TO 4 LANES RESERVE ON 438544-1

ROADWAY ID:94000120 PROJECT LENGTH: 1.946MI LANES EXIST/IMPROVED/ADDED: 2/ 2/ 2

FUND CODE	LESS THAN 2017	2017	2018	2019	2020	2021	GREATER THAN 2021	ALL YEARS
PHASE: PRELIMINARY ENGINEERING / RESPONSIBLE AGENCY: MANAGED BY FDOT								
ACSA	2,022,378	0	0	0	0	0	0	2,022,378
ACSU	15,000	0	0	0	0	0	0	15,000
EB	8,583	0	0	0	0	0	0	8,583
SA	67,428	2,397	0	0	0	0	0	69,825
SU	387,324	0	0	0	0	0	0	387,324
PHASE: RIGHT OF WAY / RESPONSIBLE AGENCY: MANAGED BY FDOT								
ACSU	0	8,245	0	0	0	0	0	8,245
SA	0	36,375	0	0	0	0	0	36,375
SU	0	142,130	177,000	45,451	0	0	0	364,581
TRIP	0	0	0	45,451	0	0	0	45,451
PHASE: ENVIRONMENTAL / RESPONSIBLE AGENCY: MANAGED BY FDOT								
SA	0	147,896	0	0	0	0	0	147,896
SU	0	37,104	0	0	0	0	0	37,104
TOTAL 431752 2	2,500,713	374,147	177,000	90,902	0	0	0	3,142,762
TOTAL PROJECT:	2,500,713	374,147	177,000	90,902	0	0	0	3,142,762

ITEM NUMBER:436617 1 PROJECT DESCRIPTION:I-95 BRIDGE DECK REPLACEMENT OVER CR-709 & FEC AND OVER TEN MILE CREEK *SIS*
 DISTRICT:04 COUNTY:ST. LUCIE TYPE OF WORK:BRIDGE REHABILITATION
 EX DESC:BRIDGE DECK REPLACEMENT ON I-95 OVER CR-709 & FEC R/R (BRDG# 940115 & 940116) AND OVER TEN MILE CREEK (BRDG# 940122 & 940123)

ROADWAY ID:94001000 PROJECT LENGTH: 3.810MI LANES EXIST/IMPROVED/ADDED: 3/ 3/ 0

FUND CODE	LESS THAN 2017	2017	2018	2019	2020	2021	GREATER THAN 2021	ALL YEARS
PHASE: PRELIMINARY ENGINEERING / RESPONSIBLE AGENCY: MANAGED BY FDOT								
BRRP	1,198,910	0	0	0	0	0	0	1,198,910
DIH	28,142	36,857	0	0	0	0	0	64,999
PHASE: RAILROAD & UTILITIES / RESPONSIBLE AGENCY: MANAGED BY FDOT								
BRRP	16,000	0	0	0	0	0	0	16,000
PHASE: CONSTRUCTION / RESPONSIBLE AGENCY: MANAGED BY FDOT								
BRRP	0	23,189,931	0	0	0	0	0	23,189,931
DIH	0	128,125	0	0	0	0	0	128,125
PHASE: CONTRACT INCENTIVES / RESPONSIBLE AGENCY: MANAGED BY FDOT								
BRRP	0	0	0	300,000	0	0	0	300,000
PHASE: ENVIRONMENTAL / RESPONSIBLE AGENCY: MANAGED BY FDOT								
BRRP	0	40,000	0	0	0	0	0	40,000
TOTAL 436617 1	1,243,052	23,394,913	0	300,000	0	0	0	24,937,965
TOTAL PROJECT:	1,243,052	23,394,913	0	300,000	0	0	0	24,937,965
TOTAL DIST: 04	3,743,765	23,769,060	177,000	390,902	0	0	0	28,080,727
TOTAL HIGHWAYS	3,743,765	23,769,060	177,000	390,902	0	0	0	28,080,727

FLORIDA DEPARTMENT OF TRANSPORTATION
 OFFICE OF WORK PROGRAM
 MPO ROLLFORWARD REPORT
 =====
TRANSIT
 =====

ST. LUCIE TPO

ITEM NUMBER:434548 1 PROJECT DESCRIPTION:ST.LUCIE COUNTY SECTION 5339 CAPITAL FOR BUS & BUS FACILITIES *NON-SIS*
 DISTRICT:04 COUNTY:ST. LUCIE TYPE OF WORK:CAPITAL FOR FIXED ROUTE
 EX DESC:ST.LUCIE CO. SECTION 5339 CAPITAL FOR BUS & BUS FACILITIES PROGRAM 16. CAPITAL FOR FIXED ROUTE

ROADWAY ID: PROJECT LENGTH: .000 LANES EXIST/IMPROVED/ADDED: 0/ 0/ 0

FUND CODE	LESS THAN 2017	2017	2018	2019	2020	2021	GREATER THAN 2021	ALL YEARS
PHASE: CAPITAL / RESPONSIBLE AGENCY: MANAGED BY ST. LUCIE COUNTY								
FTA	0	789,514	197,400	197,400	197,400	197,400	0	1,579,114
TOTAL 434548 1	0	789,514	197,400	197,400	197,400	197,400	0	1,579,114
TOTAL PROJECT:	0	789,514	197,400	197,400	197,400	197,400	0	1,579,114
TOTAL DIST: 04	8,968,195	12,954,592	2,891,400	2,891,400	2,891,400	2,891,400	0	33,488,387
TOTAL TRANSIT	8,968,195	12,954,592	2,891,400	2,891,400	2,891,400	2,891,400	0	33,488,387
GRAND TOTAL	12,756,349	37,237,752	3,068,400	3,282,302	2,891,400	2,891,400	0	62,127,603

AGENDA ITEM SUMMARY

Board/Committee:	St. Lucie TPO Board
Meeting Date:	August 3, 2016
Item Number:	9a
Item Title:	Executive Director's Performance Review
Item Origination:	St. Lucie TPO Executive Committee
UPWP Reference:	Task 1.1 - Program Management
Requested Action:	Approve the Executive Director's performance review, approve with conditions, or do not approve
Recommendation:	The TPO Executive Committee recommends that all of the TPO Board Members review the Executive Director's performance for FY 2015/16 and that the Executive Committee convene immediately following the August 3rd TPO Board Meeting to consider the compensation of the TPO Executive Director based on the results of the Performance Review.

Attachments

- TPO Attorney Memorandum
- FY 2014/15 – FY 2015/16 UPWP End Products/Projects Completed
- Grant Budget Summaries
- Completed Performance Review Forms

FY 2014/15 – FY 2015/16 UNIFIED PLANNING WORK PROGRAM (UPWP)
 END PRODUCTS/PROJECTS COMPLETED

Element	Task	End Products/Projects	Date of Completion
1. Program Administration	1.1 Program Management	2015 Legislative Priorities	December 2014
		2016 Legislative Priorities	December 2015
		2015 Annual FDOT/TPO Joint Certification Review	May 2015
		2016 Annual FDOT/TPO Joint Certification Review	May 2016
		By-Laws, Rules, and Procedures Update	February 2016
		MPO-FDOT Agreement	April 2016
		TPO Audio-Visual Production System Installation	December 2015
	1.2 UPWP Development	FY 2016/17 – FY 2017/18 Unified Planning Work Program	June 2016
2. Modeling and Data Collection, Monitoring & Management	2.1 Travel Demand Modeling	Treasure Coast Regional Planning Model V4 Development	October 2014
	2.2 Data Collection and Monitoring	Pedestrian Facility Inventory Program Annual Update	January 2015
	2.3 Traffic Count Program Management	Level of Service (LOS) Analysis System Update	December 2014
		2014 Traffic Count and LOS Report	January 2015
		2015 Traffic Count and LOS Report	January 2016
3. Recurring and Systems Planning	3.1 Long Range Transportation Planning and MAP-21 Implementation	Go2040 Long Range Transportation Plan (LRTP)	February 2016
		2040 Regional LRTP Scope of Services	February 2016
	3.2 Transit Planning	Treasure Coast Bus System Map	December 2014
		Transit Design Guidelines	May 2015
		Water Taxi Feasibility Study	June 2016
	3.3 Transportation Improvement Program (TIP)	Transit Development Plan Annual Progress Report	September 2015
		2014/15 List of Priority Projects	September 2014
		Review/Endorsement of FDOT's Five-Year Work Program	December 2014
		FY 2015/16 – FY 2019/20 TIP	June 2015
		Annual Publication of Obligated Federal Projects	June 2015
		2015/16 List of Priority Projects	August 2015
		Review/Endorsement of FDOT's Five-Year Work Program	October 2015
		FY 2016/17 – FY 2020/21 TIP	June 2016
Annual Publication of Obligated Federal Projects	June 2016		
TIP/STIP Amendments (6)	Various		

Element	Task	End Products/Projects	Date of Completion
3. Recurring and Systems Planning (continued)	3.4 Congestion Management Process (CMP)	2014 CMP Annual Report	February 2015
	3.5 Bicycle/Pedestrian/Greenway Planning	Pedestrian Facility Inventory Program Annual Update	June 2014
		Bicycle Rack Plan	December 2015
		East Coast Greenway Sign Plans	May 2016
		2015 Transportation Alternatives Program Grants	April 2015
		2016 Transportation Alternatives Program Grants	June 2016
		2014 Treasure Coast Scenic Highway Annual Report	March 2015
	2015 Treasure Coast Scenic Highway Annual Report	February 2016	
	3.6 Freight Planning	St. Lucie Freight Network Designation	August 2015
	3.7 Safety and Security Planning	Continuity of Operations Plan (COOP) Plan	December 2014
		COOP Exercise	August 2015
		Security Issue Evaluation – Avenue J Pedestrian Bridge	April 2015
		Pedestrian Safety Action Plan	February 2016
	3.8 Transportation Disadvantaged (TD) Program and Ladders of Opportunity	2014 Annual CTC Evaluation	December 2014
		TD Service Plan/Coordinated Plan Minor Update	December 2014
2015 Annual CTC Evaluation		December 2015	
TD Service Plan/Coordinated Plan Major Update		December 2015	
4. Special Project Planning	4.1 Walton Road Multimodal Improvements	Walton Road Improvements Feasibility Study	June/August 2016
	4.2 US-1 Corridor Retrofit Project	Implementation of US-1 Retrofit Projects	February 2016
		Project Identification and Prioritization-St. Lucie ATMS	June 2016
		2015 TRIP Funding-Port St. Lucie Boulevard	June 2015
		2016 TRIP Funding-Port St. Lucie Boulevard	April 2016
		St. Lucie-Martin Reginal Waterways Plan	September 2014
		St. Lucie-Martin Reginal Waterways Prioritized Projects	December 2014
	U.S. Highway 1 Bus Shelters Implementation	Various	
5.2 Intergovernmental Planning and Coordination	Savannas County Park Trail	January 2016	
Jobs Express Terminal Funding	June 2016		
6. Public Involvement, Education & Outreach	6.1 Public Involvement, Education & Outreach	Plein Air Workshops – National Academy of Sciences, Transportation Research Board Annual Meeting	January 2016
		Engaging the Silent Citizen – American Planning Association, Florida Chapter Annual Conference	September 2015
		Annual PIP Evaluation of Effectiveness	August 2015
		Title VI Program Update	April 2016

SUMMARY OF COSTS INCURRED BY UPWP TASK
 FY 2015/16-4th Quarter (Invoice #8)
 April 1, 2016 - June 30, 2016



FM No. 423604-2-14-01
 Contract #A5115
 F.A. Program No. PL-0311(52)

UPWP Task No.	UPWP Task Description	Fiscal Year 2014/15 Budget by Task	Fiscal Year 2015/16 Budget by Task	Staff Hours per Task This Quarter	Salaries Paid this Quarter	Benefitis Paid this Quarter	Expenses Paid This Quarter	Total Reimbursable Costs Incurred This Quarter	Total Reimbursable Costs Previously Billed	Total Reimbursable Costs Incurred To Date	Percentage of Budget Expended to Date
Element 1 Program Administration											
1.1	Program Management	\$275,000	\$280,000	313.5	\$7,290.85	\$3,881.33	\$37,934.49	\$49,106.67	\$505,918.14	\$555,024.81	100%
1.2	UPWP Development	\$1,000	\$16,000	50.0	\$2,440.09	\$502.58	\$0.00	\$2,942.68	\$14,057.30	\$16,999.98	100%
Element 2 Modeling and Data Collection, Monitoring & Management											
2.1	Travel Demand Modeling	\$11,000	\$1,000	-	\$0.00	\$0.00	\$0.00	\$0.00	\$12,009.29	\$12,009.29	100%
2.2	Data Collection and Monitoring	\$10,000	\$10,000	102.5	\$3,172.25	\$853.27	\$0.00	\$4,025.52	\$15,977.43	\$20,002.95	100%
2.3	Traffic Count Program Management	\$50,000	\$40,000	-	\$0.00	\$0.00	\$0.00	\$0.00	\$90,285.53	\$90,285.53	100%
Element 3 Recurring and Systems Planning											
3.1	Long Range Trans Planning & Map 21 Implementation	\$200,000	\$145,000	80.0	\$2,488.13	\$1,048.08	\$0.00	\$3,536.22	\$341,768.52	\$345,304.74	100%
3.2	Transit Planning	\$10,000	\$10,000	46.5	\$1,547.37	\$429.51	\$7.75	\$1,984.63	\$18,045.90	\$20,030.53	100%
3.3	Transportation Improvement Program (TIP)	\$25,000	\$27,000	247.5	\$7,783.18	\$2,078.67	\$495.97	\$10,357.82	\$41,893.98	\$52,251.80	100%
3.4	Congestion Management Process (CMP)	\$3,000	\$3,000	51.5	\$1,605.29	\$492.16	\$0.00	\$2,097.45	\$3,930.51	\$6,027.96	100%
3.5	Bicycle/Pedestrian/Greenway Planning	\$50,000	\$40,000	557.0	\$18,326.25	\$5,242.31	\$0.00	\$23,568.55	\$66,452.58	\$90,021.13	100%
3.6	Freight Planning	\$9,000	\$1,000	11.5	\$530.80	\$164.00	\$83.37	\$778.18	\$8,863.72	\$9,641.90	96%
3.7	Safety and Security Planning	\$16,000	\$1,000	80.0	\$2,360.72	\$712.08	\$0.00	\$3,072.80	\$13,882.20	\$16,955.00	100%
Element 4 Special Project Planning											
4.1	Walton Rd Multimodal Improvements (Begins 7/1/15)	\$0	\$50,000	108.5	\$3,532.59	\$1,138.82	\$39,682.48	\$44,353.89	\$5,249.22	\$49,603.11	99%
4.2	US-1 Corridor Retrofit Project	\$18,000	\$0	11.0	\$555.28	\$164.81	\$0.00	\$720.09	\$17,327.49	\$18,047.58	100%
Element 5 Regional & Intergovernmental Planning & Coordination											
5.1	Models of Regional Planning Cooperation	\$20,000	\$20,000	283.0	\$11,499.70	\$2,988.51	\$0.00	\$14,488.21	\$20,895.75	\$35,383.96	88%
5.2	Intergovernmental Planning & Coordination	\$10,000	\$10,000	103.5	\$3,648.95	\$1,107.76	\$115.18	\$4,871.89	\$15,094.05	\$19,965.94	100%
Element 6 Public Involvement, Education & Outreach											
6.1	Public Involvement, Education & Outreach	\$20,000	\$35,000	65.0	\$2,020.07	\$584.44	\$2,416.49	\$5,021.00	\$49,924.31	\$54,945.31	100%
TOTALS		\$728,000.00	\$689,000.00	2,111.0	\$68,801.53	\$21,388.33	\$80,735.73	\$170,925.59	\$1,241,575.89	\$1,412,501.48	100%

SUMMARY OF FTA COSTS INCURRED BY UPWP TASK
 Fiscal Year 2016 - 3rd Quarter Invoice #3
 April 1 - June 30 2016



FM No. 41373721401
 Contract #GO358
 County Grant No. 001575

UPWP Task No.	UPWP Task Description	Fiscal Year 2015/16 Budget by Task	Amended Fiscal Year 2015/16 Budget by Task	Staff Hours per Task This Quarter	Salaries Paid this Quarter	Benefitis Paid this Quarter	Expenses Paid This Quarter	Total Eligible Project Costs Incurred This Quarter	Total Eligible Project Costs Previously Incurred	Total Eligible Project Costs Incurred To Date	Percentage of Budget Expended to Date
Element 1 Program Administration											
1.1	Program Management	\$18,630	\$18,000	20.0	\$873.90	\$310.28	\$663.99	\$1,848.17	\$18,052.28	\$19,900.45	54%
Element 3 Recurring and Systems Planning											
3.1	Long Range Transportation Planning	\$60,000	\$12,000	2.0	\$101.01	\$29.98	\$11,654.25	\$11,785.24	\$60,092.72	\$71,877.96	100%
3.2	Transit Planning	\$36,657	\$6,173	81.0	\$3,369.51	\$980.12	\$0.00	\$4,349.63	\$10,979.04	\$15,328.67	36%
TOTALS		\$115,287.00	\$36,173.00	103.0	\$4,344.42	\$1,320.39	\$12,318.24	\$17,983.05	\$89,124.04	\$107,107.09	71%



SUMMARY OF COSTS INCURRED BY UPWP TASK
 Invoice #004
 April - June 2016

Project Name: Florida SRTS
 Subcontract No: UFDSP00010854

UPWP Task No.	UPWP Task Description	Budget by Task	Staff Hours per Task This Quarter	Salaries Paid this Quarter	Benefitis Paid this Quarter	Expenses Paid This Quarter	Total Reimbursable Costs Incurred This Quarter	Total Reimbursable Costs Previously Billed	Total Reimbursable Costs Incurred To Date	Percentage of Budget Expended to Date
Element 3 Recurring and Systems Planning										
3.7	Safety and Security Planning	\$65,000.00	371.0	\$11,743.92	\$3,590.35	\$5,668.68	\$21,002.95	\$43,956.46	\$64,959.41	99.94%
	TOTALS	\$65,000.00	371.0	\$11,743.92	\$3,590.35	\$5,668.68	\$21,002.95	\$43,956.46	\$64,959.41	99.94%



SUMMARY OF COSTS INCURRED BY UPWP TASK
 FY 2015/16-4th Quarter
 April 1, 2106 - June 30, 2016

FM No. 43202911401
 Contract #GO260
 CSFA 55.002

UPWP Task No.	UPWP Task Description	Fiscal Year Budget by Task	Staff Hours per Task This Quarter	Salaries Paid this Quarter	Benefitis Paid this Quarter	Expenses Paid This Quarter	Total Reimbursable Costs This Quarter	Reimbursable Costs Previously Incurred	Total Reimbursable Costs To Date	Percentage of Budget Expended to Date
Element: Recurring and Systems Planning										
Task 3.8	Transportation Disadvantaged Program	\$24,889.00	188.0	\$5,965.58	\$1,896.06	\$2,316.81	\$10,178.45	\$14,340.96	\$24,519.41	99%
	TOTALS	\$24,889.00	188.0	\$5,965.58	\$1,896.06	\$2,316.81	\$10,178.45	\$14,340.96	\$24,519.41	99%

AGENDA ITEM SUMMARY

Board/Committee:	St. Lucie TPO Board
Meeting Date:	August 3, 2016
Item Number:	9b
Item Title:	Staff Performance Bonus Program
Item Origination:	TPO Executive Committee
UPWP Reference:	Task 1.1 - Program Management
Requested Action:	Approve the 2016 Staff Performance Bonus Program, approve with conditions, or do not approve.
Staff Recommendation:	Based on the performance of the TPO staff for FY 2015/16, the TPO Executive Committee recommends approval of the 2016 Performance Bonus Program with a multiplier of 2.5 and no cost of living adjustment.

Attachments

- Staff Report

MEMORANDUM

TO: St. Lucie TPO Board

FROM: Peter Buchwald
Executive Director

DATE: July 26, 2016

SUBJECT: Staff Performance Bonus Program

BACKGROUND

In October 2013, the TPO Board adopted a performance bonus program to recognize past performance and encourage continued high performance of TPO staff members. The Staff Performance Bonus Program consists of a review of the staff member's performance annually by the TPO Executive Director after the completion of the TPO's fiscal year. The evaluation includes a review of qualitative and quantitative performance elements of the staff member's overall contributions toward the completion of the Unified Planning Work Program (UPWP) end products and projects and the objectives of the organization. The evaluation results in an overall performance score for the staff member for the previous fiscal year ranging from zero to two.

As part of the Staff Performance Bonus Program, after each fiscal year, the TPO Board approves a multiplier based on the performance of the organization in the previous fiscal year. The multiplier is applied to the staff member's overall performance score to determine the percentage of any salary increase for the staff member.

Since the adoption of the Staff Performance Bonus Program, the Board has annually approved salary increases consisting of various cost of living adjustments and the performance multiplier. The most recent salary increases for staff approved by the Board in August 2015 consisted of a performance multiplier of 1.5 with no cost of living adjustment. This resulted in a maximum salary increase for any staff member of three percent.

ANALYSIS

During FY 2015/16, the TPO staff continued to perform at an overall high performance level as documented in the FY 2014/15 – FY 2015/16 UPWP End Products/Projects Completed. Because of this continued high performance, a 2.5 performance multiplier is being recommended by the TPO Executive Director for approval by the Board. This would be multiplied by the staff member's performance score for FY 2015/16 resulting in a potential performance bonus salary increase of up to five percent for a staff member. This potential bonus salary increase would not be applied to the TPO Executive Director as salary increases for the TPO Executive Director are approved separately by the TPO Board in accordance with the Employment Agreement.

The FY 2016/17 budget that was adopted by the TPO Board in April 2016 includes an overall increase in staff salaries for FY 2016/17 of \$20,000 which is an approximately five percent line-item increase from FY 2015/16. The total salary increases for staff members for FY 2016/17 will not exceed this amount.

At their meeting on July 15th, the TPO Executive Committee reviewed the salary increases, consisting of various cost of living adjustments and performance multipliers, approved by the TPO Board as part of the Staff Performance Bonus Program since 2013. The Executive Committee then discussed the proposed 2016 performance bonus program multiplier of 2.5 and that there is no cost of living adjustment proposed for 2016 because of the current lack of significant inflation. The use of this multiplier would result in potential salary increases for the staff ranging from zero to five percent. The discussion also included comments regarding the TPO's FY 2016/17 budget accommodating staff salary adjustments, salary increases for staffs in the proposed FY 2016/17 budgets for the other local jurisdictions and agencies, and the economies of the TPO staff.

Based on the discussion, the Executive Committee approved a motion to recommend a performance multiplier of 2.5 with no cost of living adjustment for the 2016 Staff Performance Bonus Program

RECOMMENDATION

Based on the performance of the TPO staff for FY 2015/16, the TPO Executive Committee recommends approval of the 2016 performance bonus program with a multiplier of 2.5 and no cost of living adjustment.



Coco Vista Centre
466 SW Port St. Lucie Blvd, Suite 111
Port St. Lucie, Florida 34953
772-462-1593 www.stlucietpo.org

AGENDA ITEM SUMMARY

Board/Committee: St. Lucie TPO Board

Meeting Date: August 3, 2016

Item Number: 9c

Item Title: 2016/17 List of Priority Projects (LOPP)

Item Origination: For development of the St. Lucie TPO's Transportation Improvement Program (TIP) for FY 2017/18 – FY 2021/22 and the Florida Department of Transportation Work Program

UPWP Reference: Task 3.3 – TIP

Requested Action: Adopt the LOPP, adopt with conditions, or do not adopt

Staff Recommendation: Based on the recommendations of the St. Lucie TPO Advisory Committees and the consistency of the LOPP with the Go2040 Long Range Transportation Plan, it is recommended that the draft 2016/17 LOPP be adopted.

Attachments

- Staff Report
- Draft 2016/17 LOPP
- 2015/16 LOPP



Coco Vista Centre
466 SW Port St. Lucie Blvd, Suite 111
Port St. Lucie, Florida 34953
772-462-1593 www.stlucietpo.org

MEMORANDUM

TO: St. Lucie TPO Board

FROM: Peter Buchwald
Executive Director

DATE: July 25, 2016

SUBJECT: 2016/17 List of Priority Projects (LOPP)

BACKGROUND

The adoption of the LOPP is part of the annual development of the St. Lucie TPO's Transportation Improvement Program (TIP). An Informal Priority Projects Meeting was conducted with FDOT and local agency staffs on June 12th to initiate the annual TIP development process. The LOPP subsequently is developed and presented to the TPO Board for review and adoption. Upon its adoption, the LOPP is submitted to the Florida Department of Transportation District 4 (FDOT) by October 1st of each year. The projects identified in the LOPP are to be funded and included in the FDOT Work Program to the maximum extent feasible.

A draft version of the FDOT Work Program is presented for endorsement by the TPO Board typically in December of each year. Subsequently, the St. Lucie TPO's TIP for FY 2017/18 – FY 2021/22 is developed based on the LOPP and the FDOT Work Program. Upon the adoption of the TIP by the TPO Board, the annual TIP development process starts again with the Informal Priority Projects Meeting.

ANALYSIS

The draft 2016/17 LOPP is attached which incorporates comments received during the Informal Priority Projects Meeting. Significant changes from the 2015/16 LOPP, also attached, include the removal from the Master LOPP of the U.S. Highway 1 and Virginia Avenue Intersection Project and the Advanced Transportation Management System (ATMS) Phase I Project because they are programmed for construction. The remaining projects in the Master LOPP,

which are included in the Cost Feasible Plan of the recently-adopted Go2040 Long Range Transportation Plan (LRTP), were reprioritized as necessary. In addition, the status and estimated costs of the projects in the Master LOPP were updated as necessary.

Because the TPO's Congestion Management Process (CMP) will receive a Major Update during this fiscal year, there are no changes proposed to the CMP LOPP. The Transit LOPP also did not experience any changes at the recommendation of the transit provider and operator, St. Lucie County and Community Transit, respectively.

Revisions to the Transportation Alternatives (TA) LOPP include removing the Parr Drive Sidewalk Project because it is programmed for construction. This project is replaced by the North Macedo Boulevard Sidewalk Project which was prioritized by the TPO Board this past June for funding from the 2016 TA grant cycle. Other revisions to the TA LOPP reflect the adoption of the Go2040 LRTP.

At their meetings during the week of July 11th, the TPO Advisory Committees recommended the adoption of the draft 2016/17 LOPP.

RECOMMENDATION

Based on the recommendations of the St. Lucie TPO Advisory Committees and the consistency of the LOPP with the Go2040 LRTP, it is recommended that the draft 2016/17 LOPP be adopted.



DRAFT 2016/17 List of Priority Projects (LOPP) (Adopted _____)

Master List

2016/17 Priority Ranking	Major Gateway Corridor? ¹	Facility	Project Limits		Project Description	Project Status/Notes	In LRTP ² Cost Feasible Plan?	Estimated Cost	2015/16 Priority Ranking
			From	To					
1	Yes	Port St. Lucie Boulevard	Paar Drive	Darwin Boulevard	Add 2 lanes, sidewalks, bicycle lanes	PE ³ and ROW ⁴ in process	Yes	\$11,700,000 ⁵	1
2	Yes	Midway Road	Glades Cut Off Road	Selvitz Road	Add 2 lanes, sidewalks, bicycle lanes	PD&E ⁶ in process, PE and ROW to start in FY 2016/17	Yes	\$47,000,000 ⁷	2
3	Yes	Port St. Lucie Boulevard	Becker Road	Paar Drive	Add 2 lanes, sidewalks, bicycle lanes	PE to start in FY 2020/21	Yes	\$9,600,000 ⁵	5
4	Yes	Kings Highway	I-95 Overpass	St. Lucie Boulevard	Add 2 lanes, sidewalks, bicycle lanes	PE to start in FY 2017/18	Yes	\$29,520,000 ⁸	6

¹Landscape funding eligibility for capacity projects based on 2012 FDOT Landscape Policy

²LRTP: *Go2040 Long Range Transportation Plan, February 2016*

³PE: Preliminary Engineering

⁴ROW: Right-of-Way Acquisition

⁵Source: *Port St. Lucie Boulevard, Becker Road to Darwin Boulevard Project Development & Environment Study, September 2014*

⁶PD&E: Project Development & Environment Study

⁷Source: Midway Road PD&E Alternatives Public Workshop, June 2016

⁸Source: *Kings Highway Project Development & Environment Study, July 2012*

Congestion Management Process (CMP) List

(The St. Lucie TPO's allocation of urban-attributable Federal funds to CMP projects is \$300,000 - \$400,000 annually)

2016/17 Priority Ranking	Facility/Intersection	Project Description	Project Status/Notes	Estimated Cost ¹	CMP Plan ² Ranking	2015/16 Priority Ranking
1	California Boulevard at University Boulevard	Construct a roundabout		\$350,000	1	1
2	California Boulevard at Del Rio Boulevard	Construct a roundabout		\$350,000	2	2
3	St. Lucie West Boulevard at Peacock Boulevard	Extend the southbound innermost left-turn lane and incorporate signal timing adjustments		\$100,000	3	3
4	Port St. Lucie Boulevard at Floresta Boulevard	Extend westbound right-turn lane		\$350,000	4	4

¹Source of Estimated Cost: CMP Plan, unless otherwise noted

²CMP Plan: *St. Lucie Transportation Planning Organization Congestion Management Process Revised Implementation Plan, 2015*

Transit

2016/17 Priority Ranking	Facility/Equipment/Service	Project Location/Description	Is funding for Capital or Operating?	In LRTP ¹ or TDP ² ?	Estimated Cost ³	2015/16 Priority Ranking
1	Vehicle Purchases	New/replacement buses for new and expanded services as specified in TDP	Capital	Yes	\$2,000,000	1
2	Bus Stop and Park and Ride Infrastructure	Miscellaneous locations along the fixed routes with priority at transfer locations	Capital	Yes	\$75,000 (for bus shelters)	2
3	Expanded Local Services	Routes 1, 2 & 3 – Improve frequency to 30 minutes	Operating	Yes	\$1,000,000	3
4	New Bus Services	New bus service via intermodal facility along 25th Street	Operating	Yes	\$500,000	4
5	I-95 Express Regional Bus Service	To Palm Beach County	Operating	Yes	\$500,000	5
6	St. Lucie Transit Administration and Operations Facility	Centralized facility for transit operations and bus maintenance	Capital	Yes	\$9,800,000	6

¹LRTP: *Go2040 Long Range Transportation Plan, February 2016*

²TDP: *St. Lucie County FY 2015-FY 2024 Transit Development Plan Major Update, June 2014*

³Source of Estimated Cost: Tables 9-1, 9-3, 9-9, and 9-10 of TDP, unless otherwise noted

Transportation Alternatives (TA) Projects

2016/17 Priority Ranking	Score ¹	Facility	Project Limits		Project Description	Project Source	Estimated Cost ²	2015/16 Priority Ranking
			From	To				
1	41.5	North Macedo Boulevard	Selvitz Road	St. James Drive	Sidewalk-1.0 miles	2016 TA Grant Application ³	\$872,243 ⁴	7
2	34.0	Oleander Avenue	Midway Road	Market Avenue	Sidewalk-1.3 miles	LRTP ⁵	\$1,202,125	2
3	46.5	Walton Road	Lennard Road	Green River Parkway	Sidewalk-1.1 miles	LRTP	\$632,730	3
4	43.5	17th Street Sidewalk Gaps	Georgia Avenue	Avenue Q	Sidewalk-1.7 miles	LRTP	\$222,700	4
5	43.0	East Torino Parkway	Volucia Drive	Conus Street	Sidewalk-0.4 miles	LRTP	\$220,080	5
6	41.5	Selvitz Road	Milner Drive	Peachtree Boulevard	Sidewalk-0.8 miles	LRTP	\$520,397	7
7	38.5	Thornhill Drive	Bayshore Boulevard	Airosa Boulevard	Sidewalk-1.0 miles	LRTP	\$916,023	9
8	36.5	Parr Drive	Savona Boulevard	Port St. Lucie Boulevard	Sidewalk-0.8 miles	LRTP	\$529,837	10
8	36.5	29th Street Sidewalk Gaps	Avenue I	Avenue Q	Sidewalk-0.5 miles	LRTP	\$77,000	10
8	36.5	Boston Avenue	25th Street	13th Street	Sidewalk-0.8 miles	LRTP	\$123,200	10
11	35.5	Curtis Street	Prima Vista Boulevard	Floresta Drive	Sidewalk-0.5 miles	LRTP	\$710,895	14
12	34.5	Weatherbee Road	U.S. Highway 1	Oleander Avenue	Sidewalk-0.5 miles	LRTP	\$445,220	15
13	34.0	Oleander Avenue	Midway Road	Saeger Avenue	Sidewalk-1.5 miles	LRTP	\$1,323,840	16
13	34.0	Volucia Drive	Blanton Boulevard	Torino Parkway	Sidewalk-1.0 miles	LRTP	\$870,425	16
15	32.5	29th Street	Avenue Q	Avenue T	Sidewalk-0.1 miles	LRTP	\$19,7000	18
16	31.5	Alcantarra Boulevard	Port St. Lucie Boulevard	Savona Boulevard	Sidewalk-0.8 miles	LRTP	\$703,290	19
17	29.5	Floresta Drive	Port St. Lucie Boulevard	Southbend Boulevard	Sidewalk-0.6 miles	LRTP	\$964,947	20
18	28.5	Rosser Boulevard	Openview	Bamberg Street	Sidewalk-2.1 miles	LRTP	\$1,999,182	21
19		Florida East Coast Greenway/SUN Trail	Martin County Line	Downtown Fort Pierce	Multi-Use Trail	LRTP	\$6,757,225	N/R
20		Florida East Coast Greenway/SUN Trail	Downtown Fort Pierce	Indian River County Line	Multi-Use Trail	LRTP	\$3,412,760	N/R

¹Scoring is based on the St. Lucie TPO TA Project Prioritization Methodology

²Source of Estimated Cost: *Go2040 Long Range Transportation Plan, February 2016*, unless otherwise noted

³Construction funding is anticipated to be programmed in the upcoming FDOT Tentative Work Program as a result of the 2016 TA Grant Cycle

⁴Source of Estimated Cost: 2016 TA Grant Application

⁵LRTP: *Go2040 Long Range Transportation Plan, February 2016*



2015/16 List of Priority Projects (LOPP)

(Adopted August 5, 2015)

Master List

2015/16 Priority Ranking	Major Gateway Corridor? ¹	Facility	Project Limits		Project Description	Project Status/Notes	In RL RTP ² Cost Feasible Plan?	Estimated Cost	2014/15 Priority Ranking
			From	To					
1	Yes	Port St. Lucie Boulevard	Paar Drive	Darwin Boulevard	Add 2 lanes, sidewalks, bicycle lanes	PE ³ in process	Yes	\$11,700,000 ⁴	2
2	Yes	Midway Road	Glades Cut Off Road	Selvitz Road	Add 2 lanes, sidewalks, bicycle lanes	PD&E ⁵ in process	Yes	\$19,000,000 ⁶	3
3	N/A ⁷	U.S. Highway 1 Intersection	At Virginia Avenue		Construct SB right-turn lane	PE to start in FY 2016/17	Yes	\$1,537,000 ⁸	4
4	N/A	St. Lucie TPO Advanced Transportation Management System (ATMS) Phase I	U.S. Highway 1		Fiber optic infrastructure, cameras, poles, and data collection devices to connect 56 intersections	PE in process	Yes	\$3,300,000 ⁹	5
			Turnpike Feeder Road	Savanna Club Boulevard					
			Okeechobee Road (SR-70)						
			Kings Highway	U.S. Highway 1					
5	Yes	Port St. Lucie Boulevard	Becker Road	Paar Drive	Add 2 lanes, sidewalks, bicycle lanes		Yes	\$9,600,000 ⁴	6
6	Yes	Kings Highway	I-95 Overpass	St. Lucie Boulevard	Add 2 lanes, sidewalks, bicycle lanes		Yes	\$29,520,000 ¹⁰	N/R ¹¹

¹Landscape funding eligibility for capacity projects based on 2012 FDOT Landscape Policy

²RLRTP: *2035 St. Lucie/Martin Regional Long Range Transportation Plan, February 2011*

³PE: Preliminary Engineering

⁴Source: *Port St. Lucie Boulevard, Becker Road to Darwin Boulevard Project Development & Environment Study, September 2014*

⁵PD&E: Project Development & Environment Study

⁶Source: *2035 St. Lucie/Martin Regional Long Range Transportation Plan, February 2011*

⁷N/A: Not Applicable

⁸Source: *St. Lucie TPO Transportation Improvement Program FY 2015/16 – FY 2019/20*

⁹Source: *Advanced Transportation Management System (ATMS) Master Plan for St. Lucie County, February 2013*

¹⁰Source: *Kings Highway Project Development & Environment Study, July 2012*

¹¹N/R: Not Ranked

Congestion Management Process (CMP) List

(The St. Lucie TPO's allocation of urban-attributable Federal funds to CMP projects is \$300,000 - \$400,000 annually)

2015/16 Priority Ranking	Facility/Intersection	Project Description	Project Status/Notes	Estimated Cost¹	CMP Plan² Ranking	2014/15 Priority Ranking
1	California Boulevard at University Boulevard	Construct a roundabout		\$350,000	1	1
2	California Boulevard at Del Rio Boulevard	Construct a roundabout		\$350,000	2	2
3	St. Lucie West Boulevard at Peacock Boulevard	Extend the southbound innermost left-turn lane and incorporate signal timing adjustments		\$100,000	3	3
4	Port St. Lucie Boulevard at Floresta Boulevard	Extend westbound right-turn lane		\$350,000	4	4

¹Source of Estimated Cost: CMP Plan, unless otherwise noted

²CMP Plan: *St. Lucie Transportation Planning Organization Congestion Management Process Revised Implementation Plan, 2015*

Transit

2015/16 Priority Ranking	Facility/Equipment/Service	Project Location/Description	Is funding for Capital or Operating?	In RLRTP ¹ or TDP ² ?	Estimated Cost ³	2014/15 Priority Ranking
1	Vehicle Purchases	New/replacement buses for new and expanded services as specified in TDP	Capital	Yes	\$2,000,000	1
2	Bus Stop and Park and Ride Infrastructure	Miscellaneous locations along the fixed routes with priority at transfer locations	Capital	Yes	\$75,000 (for bus shelters)	7
3	Expanded Local Services	Routes 1, 2 & 3 - Improve frequency to 30 minutes	Operating	Yes	\$1,000,000	4
4	New Bus Services	New bus service via intermodal facility along 25th Street	Operating	Yes	\$500,000	N/R ⁴
5	I-95 Express Regional Bus Service	To Palm Beach County	Operating	Yes	\$500,000	6
6	St. Lucie Transit Administration and Operations Facility	Centralized facility for transit operations and bus maintenance	Capital	Yes	\$9,800,000	8

¹RLRTP: 2035 St. Lucie/Martin Regional Long Range Transportation Plan, February 2011

²TDP: St. Lucie County FY 2015-FY 2024 Transit Development Plan Major Update, June 2014

³Source of Estimated Cost: Tables 9-1, 9-3, 9-9, and 9-10 of TDP, unless otherwise noted

⁴N/R: Not Ranked

Transportation Alternatives (TA) Projects

2015/16 Priority Ranking	Score ¹	Facility	Project Limits		Project Description	Project Source	Estimated Cost ²	2014/15 Priority Ranking
			From	To				
1	40.0	Parr Drive	Port St. Lucie Boulevard	Darwin Boulevard	Sidewalk-1.0 miles	2015 TA Grant Application ³	\$569,984	8
2	34.0	Oleander Avenue	Midway Road	Market Avenue	Sidewalk-1.3 miles	2015 TA Grant Application ⁴	\$917,653	16
3	46.5	Walton Road	Lennard Road	Green River Parkway	Sidewalk-1.1 miles	St. Lucie County School District	\$483,000	2
4	43.5	17th Street Sidewalk Gaps	Georgia Avenue	Avenue Q	Sidewalk-1.7 miles	2010/11 LOPP	\$170,000 ⁵	3
5	43.0	East Torino Parkway	Volucia Drive	Conus Street	Sidewalk-0.4 miles	St. Lucie County School District	\$168,000	4
6	42.0	East Torino Parkway	Peacock Apartments	C-106 Canal	Sidewalk-0.3 miles	2013 TA Grant Application	\$207,730	5
7	41.5	North Macedo Boulevard	Selvitz Road	St. James Drive	Sidewalk-1.0 miles	Port St. Lucie Sidewalk List	\$525,220	6
7	41.5	Selvitz Road	Milner Drive	Peachtree Boulevard	Sidewalk-0.8 miles	2010/11 LOPP	\$337,920 ⁶	6
9	38.5	Thornhill Drive	Bayshore Boulevard	Airosa Boulevard	Sidewalk-1.0 miles	Port St. Lucie Sidewalk List	\$594,820	9
10	36.5	Parr Drive	Savona Boulevard	Port St. Lucie Boulevard	Sidewalk-0.8 miles	Port St. Lucie Sidewalk List	\$344,050	10
10	36.5	29th Street Sidewalk Gaps	Avenue I	Avenue Q	Sidewalk-0.5 miles	2010/11 LOPP	\$50,000 ⁵	10
10	36.5	Boston Avenue	25th Street	13th Street	Sidewalk-0.8 miles	2010/11 LOPP	\$80,000 ⁵	10
13	36	Floresta Drive	Port St. Lucie Boulevard	Streamlet Avenue	Sidewalk-1.0 mile	Port St. Lucie Sidewalk List #7	\$759,730	13
14	35.5	Curtis Street	Prima Vista Boulevard	Floresta Drive	Sidewalk-0.5 miles	Port St. Lucie Sidewalk List	\$461,620	14
15	34.5	Weatherbee Road	U.S. Highway 1	Oleander Avenue	Sidewalk-0.5 miles	St. Lucie County School District	\$226,000	15
16	34.0	Oleander Avenue	Midway Road	Saeger Avenue	Sidewalk-1.5 miles	St. Lucie County School District	\$672,000	16
16	34.0	Volucia Drive	Blanton Boulevard	Torino Parkway	Sidewalk-1.0 miles	St. Lucie County School District	\$441,840	16
18	32.5	29th Street	Avenue Q	Avenue T	Sidewalk-0.1 miles	2010/11 LOPP	\$10,000 ⁵	19
19	31.5	Alcantarra Boulevard	Port St. Lucie Boulevard	Savona Boulevard	Sidewalk-0.8 miles	St. Lucie County School District	\$357,000	20
20	29.5	Floresta Drive	Port St. Lucie Boulevard	Southbend Boulevard	Sidewalk-0.6 miles	Port St. Lucie Sidewalk List #8	\$489,821	21
21	28.5	Rosser Boulevard	Newport Isle	Bamberg Street	Sidewalk-2.1 miles	Port St. Lucie Sidewalk List #1	\$1,014,813	22
22	25.5	Import Drive	Gatlin Boulevard	Savage Boulevard	Sidewalk-2.0 miles	Port St. Lucie Sidewalk List #3	\$1,255,161	23

2015/16 Priority Ranking	Score ¹	Facility	Project Limits		Project Description	Project Source	Estimated Cost ²	2014/15 Priority Ranking
			From	To				
23	21.5	Paar Drive	Bamberg Street	Savona Boulevard	Sidewalk-0.8 miles	Port St. Lucie Sidewalk List #2	\$1,014,728	24
23	21.5	Southbend Boulevard	Oakridge Drive	Eagle Drive	Bridge and Sidewalk- 0.2 miles	Port St. Lucie Sidewalk List #13	\$1,526,084	24
25	20.5	Savage Boulevard	Import Drive	Gatlin Boulevard	Sidewalk-1.7 miles	Port St. Lucie Sidewalk List #4	\$1,293,199	26
25	20.5	Bayshore Boulevard	Mountwell Street	Port St. Lucie Boulevard	Sidewalk-0.8 miles	Port St. Lucie Sidewalk List #6	\$695,496	26
25	20.5	Emil Avenue	Oleander Avenue	U.S. Highway 1	Sidewalk-0.4 miles	2014 TA Grant Application	\$347,487	26
28	20.0	Traffic Signal Preemption Technology	Various	Various	50 Intersections 55 Fire/EMS vehicles	St. Lucie County Fire District	\$750,000	29
29	19.5	Oakridge Drive	Southbend Drive	Mountwell Street	Sidewalk-0.8 miles	Port St. Lucie Sidewalk List #5	\$736,575	30
29	19.5	Tiffany Avenue	Lennard Road	Grand Drive	Sidewalk-0.9 miles	Port St. Lucie Sidewalk List #9	\$365,843	30
29	19.5	Selvitz Road	Floresta Drive	Bayshore Boulevard	Sidewalk-0.5 miles	Port St. Lucie Sidewalk List #10	\$962,435	30
29	19.5	Cashmere Boulevard	Charter School	Westgate K-8 School	Sidewalk-1.0 miles	Port St. Lucie Sidewalk List #11	\$590,464	30
29	19.5	Idol Drive	Charter School	Savona Boulevard	Sidewalk-0.7 miles	Port St. Lucie Sidewalk List #12	\$483,037	30
34	17.0	Bicycle Facilities Improvement Program	Various	Various	Install various bicycle facilities	2011 TE Grant Application	\$401,353	35
35	5.0	West Cedar Pedestrian Mall	2nd Street	FEC Railroad	Streetscape improvements	2011 TE Grant Application	\$440,756	36

¹Scoring is based on the St. Lucie TPO TA Project Prioritization Methodology

²Source of Estimated Cost: Project Source, unless otherwise noted

³Construction funding is anticipated to be fully programmed in the upcoming FDOT Tentative Work Program as a result of the 2015 TA Grant Cycle

⁴Construction funding was partially awarded as a result of the 2015 TA Grant Cycle

⁵Estimated cost is based on an assumed cost of \$100,000 per mile

⁶Source: City of Port St. Lucie Engineering Department



Coco Vista Centre
466 SW Port St. Lucie Blvd, Suite 111
Port St. Lucie, FL 34953
772-462-1593 www.stlucietpo.org

AGENDA ITEM SUMMARY

Board/Committee: St. Lucie TPO Board

Meeting Date: August 3, 2016

Item Number: 9d

Item Title: Walton Road Multimodal Improvements Feasibility Study

Item Origination: Unified Planning Work Program (UPWP)

UPWP Reference: Task 3.5 – Bicycle/Pedestrian/Greenway Planning

Requested Action: Accept the study, accept with conditions, or do not accept

Staff Recommendation: Based on the study evaluating the feasibility of multimodal improvements to Walton Road, the comments received to date being addressed in the revised version, and the recommendations of the TPO Advisory Committees, it is recommended that the revised Walton Road Multimodal Improvements Feasibility Study be accepted.

Attachments

- Staff Report
- Revised Walton Road Multimodal Improvements Feasibility Study



Coco Vista Centre
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Port St. Lucie, FL 34953
772-462-1593 www.stlucietpo.org

MEMORANDUM

TO: St. Lucie TPO Board

THROUGH: Peter Buchwald
Executive Director

FROM: Ed DeFini
Bicycle-Pedestrian Program Manager

DATE: July 26, 2016

SUBJECT: Walton Road Multimodal Improvements Feasibility Study

BACKGROUND

Walton Road from Lennard Road to Indian River Drive is a substandard roadway facility because it does not include shoulders or any pedestrian or bicycle facilities. The FY 2014/15 – FY 2015/16 Unified Planning Work Program (UPWP) for the St. Lucie TPO included Task 4.1, *Walton Road Multimodal Improvements*. The objective of Task 4.1 was to conduct a feasibility study of potential multimodal improvements to this segment of Walton Road. The final presentations of the Walton Road Multimodal Improvements Feasibility Study were continued in Task 3.5, *Bicycle/Pedestrian/Greenway Planning*, of the FY 2016/17 - FY 2017/18 UPWP.

The study was completed by Stanley Consultants, Inc. who is one of the TPO's General Planning Consultants. Stakeholder's input and planning agency consensus was obtained during the study through continuous and cooperative efforts, and the planning activities were coordinated with St. Lucie County and the City of Port St. Lucie.

ANALYSIS

The Walton Road Multimodal Improvements Feasibility Study evaluates potential widening and multimodal improvements with respect to environmental, socio-cultural, and engineering factors. The study contains analyses of the existing conditions, including current vehicle, bicycle, and

pedestrian volumes, and the potential impacts and environmental concerns, including fatal flaws, associated with several improvement alternatives. In addition, kayak crossing options were evaluated in order to improve connectivity for crossing Walton Road from the kayak launch located in Savannas Preserve State Park south of Walton Road to the portion of the park located north of Walton Road. The study also contains the estimated costs for each of the improvement alternatives.

At the TPO advisory committee meetings conducted in May, concerns were identified regarding stormwater conveyance issues that could result in major section redesigns and cost increases significant enough to make the project infeasible. Other areas of concern that were identified included connectivity among project segments so that users would not be required to cross Walton Road to remain on the multi-use path, providing different options for the typical sections, and the infeasibility of a kayak portage option over Walton Road.

Potential concerns identified by the TPO Board at its June meeting included the safety hazards associated with bike lanes that are not buffered from traffic and with a lack of shoulders, curbs, and gutters; the use of arched culverts and the associated maintenance for the kayak crossing; the life span and maintenance costs of the boardwalk alternative; the potential for a bridge to replace the section between Green River Parkway and Indian River Drive; and environmental permitting costs.

Subsequently, the study was revised to address the concerns identified by the TPO Board and Advisory Committees. The revised study provides different options for the typical sections and updated cost estimates that include approximate permitting fees. Life span and maintenance costs of the boardwalk structure alternative on the north side of Segment C are now included in the study. In addition, it is identified that a shared-use path on the south side of Segment C will not fit in the right-of-way. It further is identified that buffered bicycle lanes also will not fit in the right-of-way and their presence in Segment C would be inconsistent with the existing bike lanes in Segment A which are not buffered.

The kayak portage option of carrying a kayak over the road has been removed from Segment C in favor of the culvert option that allows the paddling of a kayak under the road. Alternative C1 was widened to accommodate a swale, and the cost estimates include elevating the roadway to facilitate drainage. Alternatives B2 and C2 now contain curb and gutter designs, and a bridge design is provided as Alternative C4.

At their meetings in July, the TPO Advisory Committees reviewed and accepted the revised version of the study with additional comments pertaining to the

potential for mitigation credits from the improvement of the stormwater culverts, the need for a variance from St. Lucie County for the proposed lane widths, and the addition of an animal crossing in one of the stormwater culverts. The study was revised to incorporate these comments.

RECOMMENDATION

Based on the study evaluating the feasibility of multimodal improvements to Walton Road, the comments received to date being addressed in the revised version, and the recommendations of the TPO Advisory Committees, it is recommended that the revised Walton Road Multimodal Improvements Feasibility Study be accepted.

Walton Road Multimodal Improvements Preliminary Report

St. Lucie Transportation Planning Organization

Prepared for:



St. Lucie Transportation
Planning
Organization

Prepared by:



June 2016



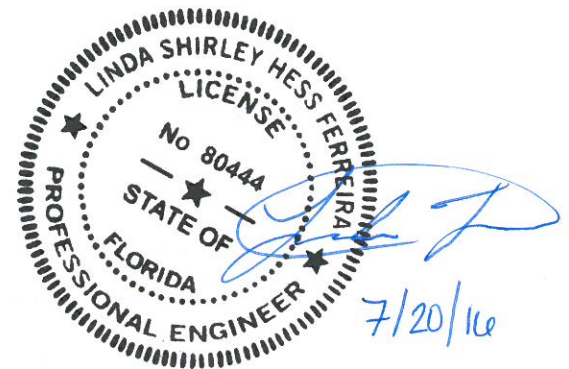
Walton Road Multimodal Improvements Feasibility Study

From Lennard Road to Indian River Drive

Prepared for:

St. Lucie Transportation Planning Organization

June 2016



Stanley Consultants, Inc.

1641 Worthington Road, Suite 400
West Palm Beach, FL 33409
Florida Engineering Cert. No. 1978

Linda Ferreira, P.E.
Professional Engineer License No. 80444



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Executive Summary

The St. Lucie Transportation Planning Organization (TPO) retained Stanley Consultants, Inc. to complete this preliminary feasibility study to evaluate multimodal improvements to the Walton Road corridor from Lennard Road to Indian River Drive. Walton Road is located within the limits of the City of Port St. Lucie and is owned and maintained by St. Lucie County. A demand for multimodal facilities exist in the surrounding study area due to residential and commercial land use, St. Lucie Civic Center, St. Lucie Medical Center, Savannas Preserve State Park, Woodstork Trail, existing off road shared use paths, and the National East Coast Greenway trail.

The multimodal improvements are consistent with local transportation plans. The FY 2014/15 – FY 2015/16 Unified Planning Work Program (UPWP) for the St. Lucie TPO includes Task 4.1, Walton Road Multimodal Improvements. Sidewalk improvements to the Walton Road corridor from Lennard Road to SE Green River Parkway are included in the list of priority projects (LOPP), were identified as part of the Walk-Bike needs and network improvements, and included in the cost feasible plan for the recently adopted Go2040 St. Lucie TPO Long Range Transportation Plan (LRTP). Completing the off road shared use path missing link from Lennard Road to SE Green River Parkway was included in the recommended preferred alternative contained in the Bicycle/Pedestrian Corridor Study Area of Interest Analysis completed by the St. Lucie TPO in 2011.

The entire study corridor spans approximately 1.9 miles with the roadway right of way (ROW) width varying from 50 feet to 150 feet. The study begins at Lennard Road as a four-lane roadway with bike lanes, sidewalk and an off road shared use path for approximately 600 feet and then transitions into a two-lane roadway with no paved shoulders or bicycle facilities all the way to Indian River Drive at the end of the study. Sidewalks, bike lanes or shared use paths are not present between SE Scenic Park Street and Indian River Drive.

The study corridor was divided into three segments based on the existing multimodal facilities, ROW width and recommended alternatives. Segment A is a four-lane divided roadway starting at Lennard Road and terminating 800 feet east of the intersection. This segment currently contains multimodal facilities on both sides of Walton Road and is the only segment with a four-lane typical section with a divided median. Segment B starts at the end of Segment A and ends at SE Green River Parkway, with varying ROW width. The existing land use is residential, with vacant parcels, a private hospital to the south, and the Savannas Preserve State Park to the north. Segment C is a two-lane rural roadway from SE Green River Parkway to Indian River Drive. Segment C has a 50-foot ROW width, guardrail through the Savannas Preserve State Park, and crosses at the FEC Railroad.

An evaluation of existing traffic counts and future volumes for Walton Road was completed to determine if roadway widening would be required in the future. Based on the planning tables found in the 2013 Florida Department of Transportation Quality and Level of Service Handbook, Walton Road is expected to have a Level of Service C through the year 2040 as a two lane roadway and widening will not be necessary.

The alternative roadway typical sections presented in this study would require a variance from St. Lucie County to reduce roadway lane width to eleven feet. The following alternatives were evaluated in this preliminary study:



- **Segment A**
 - Existing multimodal features are acceptable therefore no improvements are proposed.
- **Segment B**
 - **Alternative B1** - Extend sidewalk on north side of Walton Road from SE Scenic Park Street to Green River Parkway, extend shared use path from east of SE Rainer Road to Green River Parkway, add on-street bike lanes in both directions. Minimum ROW width required for this typical section is 65 feet.
 - **Alternative B2** Roadway reconstruction – reconstruct roadway with curb and gutter, extend sidewalk on north side of Walton Road from SE Scenic Park Street to Green River Parkway, extend shared use path from east of SE Rainer Road to Green River Parkway, add on-street bike lanes in both directions. Minimum ROW width required for this typical section is 65 feet
 - Both Alternative B1 and B2 will require ROW from the vacant school board parcel.
- **Segment C**
 - **Alternative C1** - Add sidewalk on north side of Walton Road from Green River Parkway to Indian River Drive, add on-street bike lanes in both directions. Minimum ROW width required for this typical section would be dependent on stormwater swale design and permitting requirement to meet water quality standards.
 - **Alternative C2** Roadway reconstruction – reconstruct the 2 lane roadway with curb and gutter, piped drainage system, sidewalk on north side of Walton Road from Green River Parkway to Indian River Drive, add on-street bike lanes in both directions.
 - **Alternative C3** No roadway improvements – no roadway improvements but provide a separate 10-foot wide elevated shared use boardwalk structure. Required ROW would be dependent on guardrail replacement.
 - **Alternative C4** Bridge roadway – construct a new bridge on Walton Road from east of Green River Parkway over Savannas Preserve State Park to west of the FEC railroad crossing. Sidewalk and paved shoulders would be added.
 - All alternatives for Segment C will require ROW from Savannas Preserve State Park.

Additional options for Segment C:

Kayak crossing options were evaluated for this segment in order to improve connectivity from the kayak launch south of Walton Road to the Savannas Preserve State Park to the north.

- Kayak Crossing Under Walton Road – the option of adding an under road crossing for kayaks and other non-motorized watercrafts through an arch culvert. Reconstruction and raising of Walton Road would be needed to meet kayak vertical clearance requirements.
- Kayak Crossing Over Walton Road – the option of allowing multimodal water users to cross over Walton Road with a kayak portage. The alternative would require a mid-block pedestrian crossing signal and adequate ingress and egress from the waterway.



Currently in Segment B one portion of the ROW is restricted to 50 feet due to Savannas Preserve State Park to the north and a vacant parcel owned by the St. Lucie County School Board. Based on coordination with the St. Lucie County School Board, the Board may be open to donating ROW adjacent to Walton Road to add off-road paths or sidewalks.

In Segment C, if mitigation is required, it is recommended to use a mitigation bank. Moreover, it is recommended that pre-application meetings be conducted with South Florida Water Management District (SFWMD) and the US Army Corps (land owners) in order to ensure that the alternative will avoid and minimize impacts.

Estimated costs for the recommended alternatives is listed in the table below. The cost estimate for Segment C includes an estimated \$145,000 cost for FEC railroad crossing improvements based on previous TIP projects within south Florida. The alternatives for Segment C also includes a \$3,000 permit fee and under Walton road kayak option.

Segment	Alternative	Total Cost
Segment A	Maintain existing multimodal features	None
Segment B	B1. Bike lanes, sidewalk, multiuse path	\$ 1,980,000
	B2. Curb and gutter – bike lanes, sidewalk, multiuse path	\$ 6,980,000
Segment C	C1. Add bike lanes & sidewalk	\$ 3,100,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 900,000
	Total Alternative C1 Cost	\$ 4,020,000
	C2. Roadway reconstruction with curb and gutter – add bike lanes & sidewalk	\$ 6,500,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 900,000
	Total Alternative C2 Cost	\$ 7,420,000
	C3. No roadway improvements – add raised Boardwalk*	\$ 1,310,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 1,750,000
	Total Alternative C3 Cost	\$ 3,040,000
	C4. Bridge roadway – add sidewalk and paved shoulders	\$ 43,950,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 1,750,000
	Total Alternative C4 Cost	\$ 44,900,000
*Does not include \$72,000 yearly maintenance cost		

These recommended improvements would enhance pedestrian mobility, provide safety benefits to both bicyclists and pedestrians as well as provide potential connections to future trails.



1.0 Introduction

The St. Lucie Transportation Planning Organization (TPO) retained Stanley Consultants, Inc. to complete this preliminary report to evaluate multimodal improvements to increase connectivity along the Walton Road corridor from Lennard Road to Indian River Drive. The study corridor is approximately 1.9 miles, of which 1.5 miles lack existing pedestrian or bicycle facilities.

This preliminary report is consistent with several TPO transportation plans. The study corridor is identified in the adopted Go2040 St. Lucie TPO Long Range Transportation Plan (LRTP) as part of the TPO's Walk-Bike Needs and Network Improvements and Cost Feasible Plan. The FY 2014/15 – FY 2015/16 St. Lucie TPO Unified Planning Work Program (UPWP) includes Walton Road Multimodal Improvements as a task. The corridor is also listed as second in the 2014/15 priority ranking and third on the 2015/16 list of priority projects (LOPP) as a Transportation Alternatives (TA) project for adding sidewalks. Furthermore, completing the missing link of an off-road shared use path from Lennard Road to SE Green River Parkway was included with the recommended alternative in the Bicycle/Pedestrian Corridor Study Area of Interest Analysis completed by the St. Lucie TPO in 2011.

1.1 Study Area

Walton Road from Lennard Road to Indian River Drive is located within the City of Port St. Lucie and is owned and maintained by St. Lucie County. Walton Road is classified as a Minor Arterial Urban roadway and provides the only roadway, pedestrian or bicycle link between US 1 and Indian River Drive between Midway Road (6 miles north) and Jensen Beach Blvd (4 miles south).

The study begins at Lennard Road as a four-lane section for approximately 600 feet and transitions into a two-lane section with no paved shoulders or bicycle facilities all the way to Indian River Drive (the end of the study), with a roadway right of way (ROW) width varying from 50 feet to 150 feet.

The surrounding land use is predominantly residential and includes the Savannas Preserve State Park, a convenience store, churches, a private hospital, vacant school board property, and the Florida East Coast Railway (FECR) crossing at the east end of the project. The St. Lucie County School District owns a vacant property on the southeast corner of Walton Road and SE Belcrest Street. The Port St. Lucie Hospital (private hospital) is located on the southeast corner of Walton Road and SE Grand Drive.

Savannas Preserve State Park entrance is located on Walton Road within the study segment between SE Belcrest Street and SE Grand Drive. This park entrance provides access for hikers and bicyclists. The park contains over eight miles of multi-use trails for hiking, bicycling and horseback riding. The Greater Savanna Preserve Natural Area stretches for more than 10 miles from Ft. Pierce to Jensen Beach and it is the most intact remnant of Florida's east coast savannas.

The overall study area map is shown in **Figure 1.1**. The study corridor has several multimodal trip generators nearby. There is a large residential community, St. Lucie Civic Center, St. Lucie Medical Center, Port St. Lucie High School, Sandhill Crane Park, Wood Stork Trail, Savannas Preserve State Park hiking and kayak trails, Green River Parkway Trail, connections to the beach via Indian River Drive, and the National East Coast Greenway Trail.

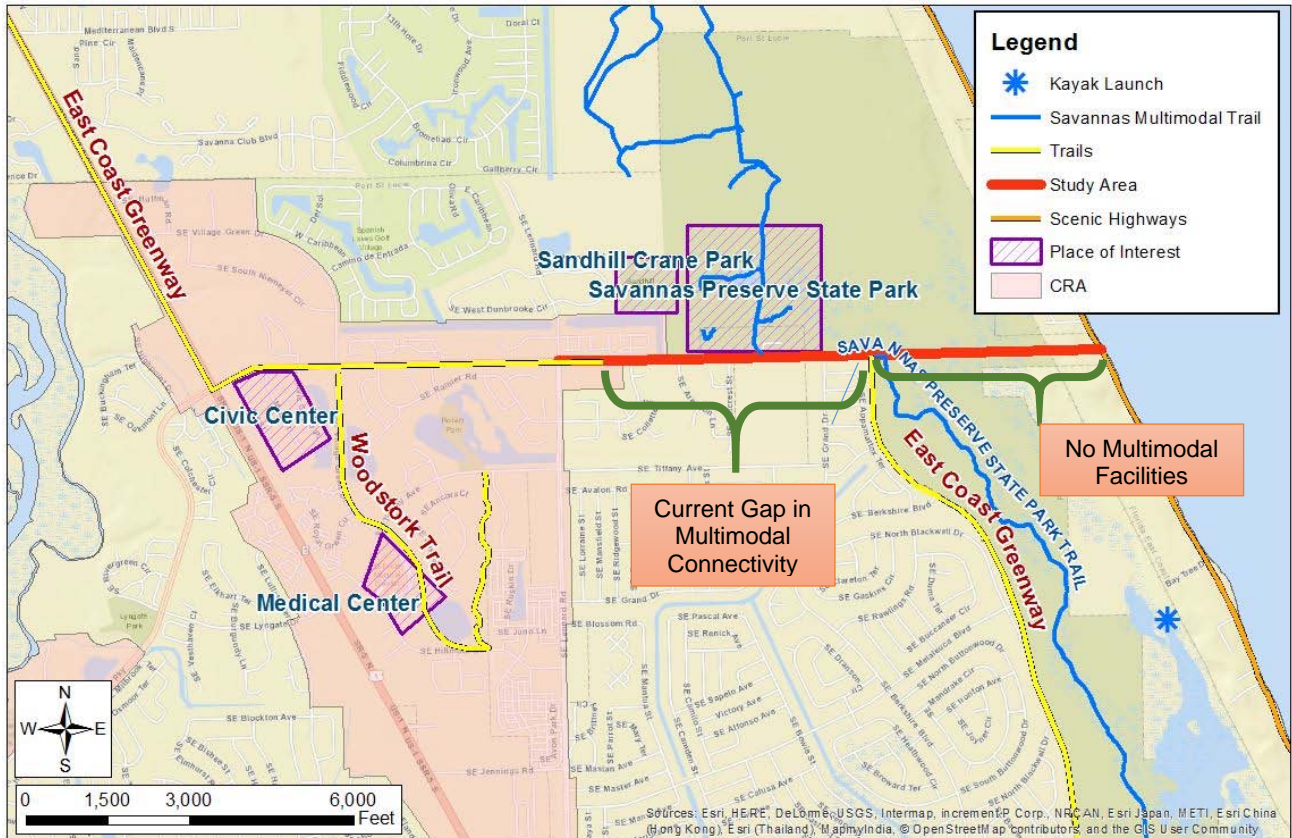


Figure 1.1 Overall Study Area

The National East Coast Greenway Trail is a 3,000-mile trail linking major cities along the Atlantic coastline from Maine to Florida. Within the Walton Road study segment, the East Coast Greenway Trail follows a variety of sidewalks and shared use paths. However, a multimodal gap exists and the national trail must utilize the two-lane rural roadside grass shoulder as the pathway until reaching the south connection that follows the Green River Parkway Trail. This study identifies improvements to fill the gap in the trail and complete other local trail and multimodal network connectivity.

The study corridor was divided into three segments based on the existing conditions and potential alternative solutions. These segments are described below and illustrated in **Figure 1.2**.

- **Segment A:** Begins at the intersection of Lennard Road and extends 800 feet east. This is where the four-lane divided roadway transitions to the two-lane rural section. The existing roadway segment contains multimodal facilities on both sides of Walton Road and is the only segment with a divided four-lane typical section.
- **Segment B:** Begins 800 feet east of Lennard Road and ends at SE Green River Parkway for a total length of 0.94 miles. This two-lane rural roadway has a ROW width from 50 ft to 150 ft. The existing roadway segment contains one sidewalk multimodal facility on the north side of Walton Road from the segment beginning to SE Scenic Park Street.



- **Segment C:** Begins at SE Green River Parkway and ends at Indian River Drive for a total length of 0.8 miles. This two-lane rural roadway has a ROW width that varies from 50 ft to 100 ft. The existing roadway segment contains no multimodal facilities.

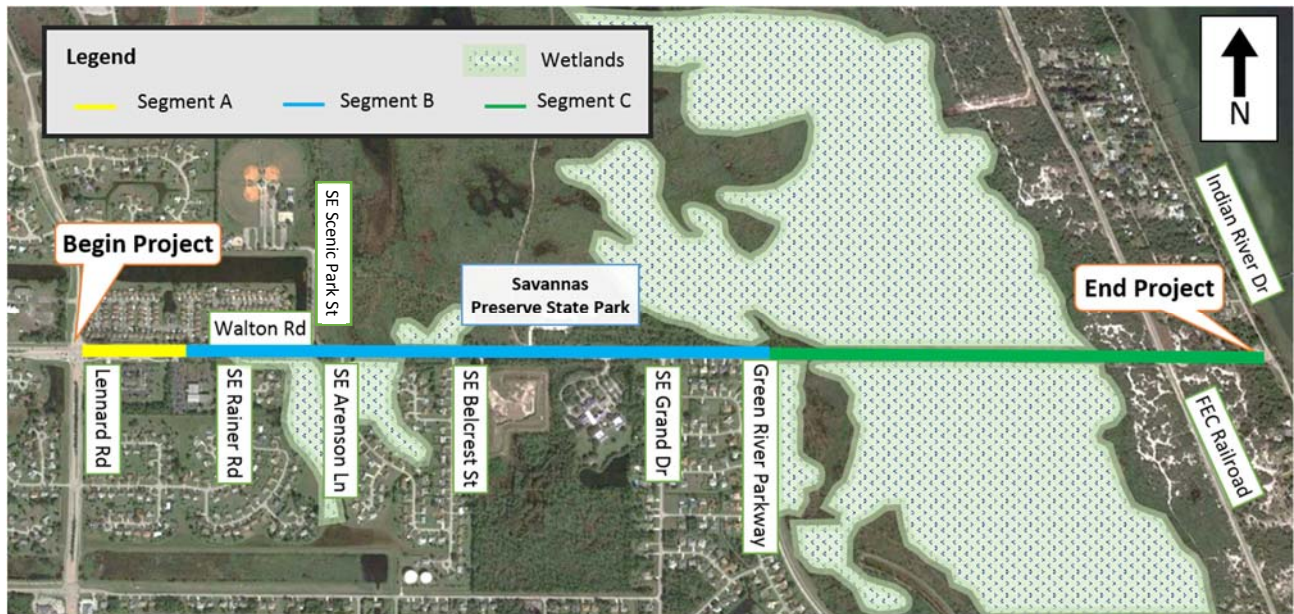


Figure 1.2 Project Segment Map

1.2 Study Purpose and Need

The purpose of this study is to evaluate the feasibility of adding multimodal improvements along the Walton Road corridor between Lennard Road and Indian River Drive to improve multimodal connectivity, pedestrian and bicyclist safety.

There are no existing bike lanes or paved shoulders on the two-lane portion of Walton Road between Lennard Road and Indian River Drive. Several multimodal trails and pathways end at Walton Road due to the lack of multimodal facilities. Vehicles and multimodal users must share the roadway travel lanes or roadside within the two-lane rural section.

An existing twelve foot wide off-road shared use path begins west of the study corridor at US-1 and meanders along the south side of Walton Road stopping at the driveway access to Highpoint Community Church between Lennard Road and SE Rainer Road. **Figure 1.3** shows the end of the path within the study corridor on the east side of the Highpoint Community Church driveway. A need exists to connect this shared use path with the paved trail at the southeast corner of Walton Road and SE Green River Parkway. This connection would provide a cohesive, safe facility for the East Coast Greenway.



Figure 1.3 End of Shared Use Path



According to the East Coast Greenway website, the section of the trail on Walton Road from Lennard Road to SE Green River Parkway is described as “on-road routing” until it joins the Green River Parkway Trail. **Figure 1.4** shows the Green River Parkway Trail termini at Walton Road. A screenshot from the website showing the East Coast Greenway trail through the study corridor is included in **Appendix D**.



Figure 1.4 End of Green River Parkway Trail

Within the study corridor an existing six-foot wide sidewalk, shown in **Figure 1.5**, is located on the north side of Walton Road from Lennard Road to SE Scenic Park Street leading north to Sandhill Crane Park which is owned by the City of Port St. Lucie. There is a need to continue the sidewalk east to the Savannas Preserve State Park entrance and also provide a safe pedestrian connection to the existing paved Green River Parkway Trail.



Figure 1.5 Sidewalk on North

Walton Road crosses the Savannas Preserve State Park kayakTrail, designated for non-motorized recreational water craft. There is a kayak launch located south of the study area off of Riverview Drive, west of the FEC railroad. The kayak trail has a gap at Walton Road due to the lack of an over-the-road kayak portage or an under-the-road kayak culvert crossing. This multimodal feasibility study evaluates kayak alternatives in **Section 3.7** in order to improve connectivity of the kayak launch across Walton Road.



2.0 Existing Conditions Analysis

2.1 Physical

Assessing existing conditions along the corridor included data collection and analysis, office and field reviews and documenting deficiencies. Plans were requested and received from St. Lucie County Engineering Division for the Walton Road Widening (County Project No. 05-51) showing the typical section, sidewalk details and improvements east of Lennard Road. A field review was conducted on Thursday, March 24, 2016 to document existing conditions, identify deficiencies and verify information from the office review.

The existing ROW width varies along the Walton Road from 50 feet to 150 feet. Between Lennard Road and SE Scenic Park Street the existing ROW width is 150 feet and narrows to 100 feet just east of SE Belcrest Street. Adjacent to the parcels owned by St. Lucie County School District and the Savannahs Preserve State Park the existing ROW width is 50 feet and widens to 65 feet adjacent to Port St. Lucie Hospital, Inc. The existing ROW width is 80 feet west of SE Grand Drive to the east of SE Green River Parkway and narrows to 50 feet through the Savannahs Preserve State Park. The existing ROW width is 100 feet from the FEC railroad crossing to Indian River Drive. The variation of ROW width is illustrated on the next page in **Figure 2.1**.

An existing raised median on Walton Road is present only within the four-lane section east of Lennard Road. The existing off-road shared use path on the south side is in good condition. Six foot wide concrete sidewalk exists along the north side between the beginning of the project at Lennard Road to SE Scenic Park Street and was found to be in good condition.

In the two-lane section, the travel lanes were measured and found to be 11 feet wide for a total paved roadway width of 22 feet. The pavement condition for the two-lane section (Segment B & C) was found to be in good condition based on a full depth resurfacing of the existing roadway completed as a maintenance project by St. Lucie County in 2015.

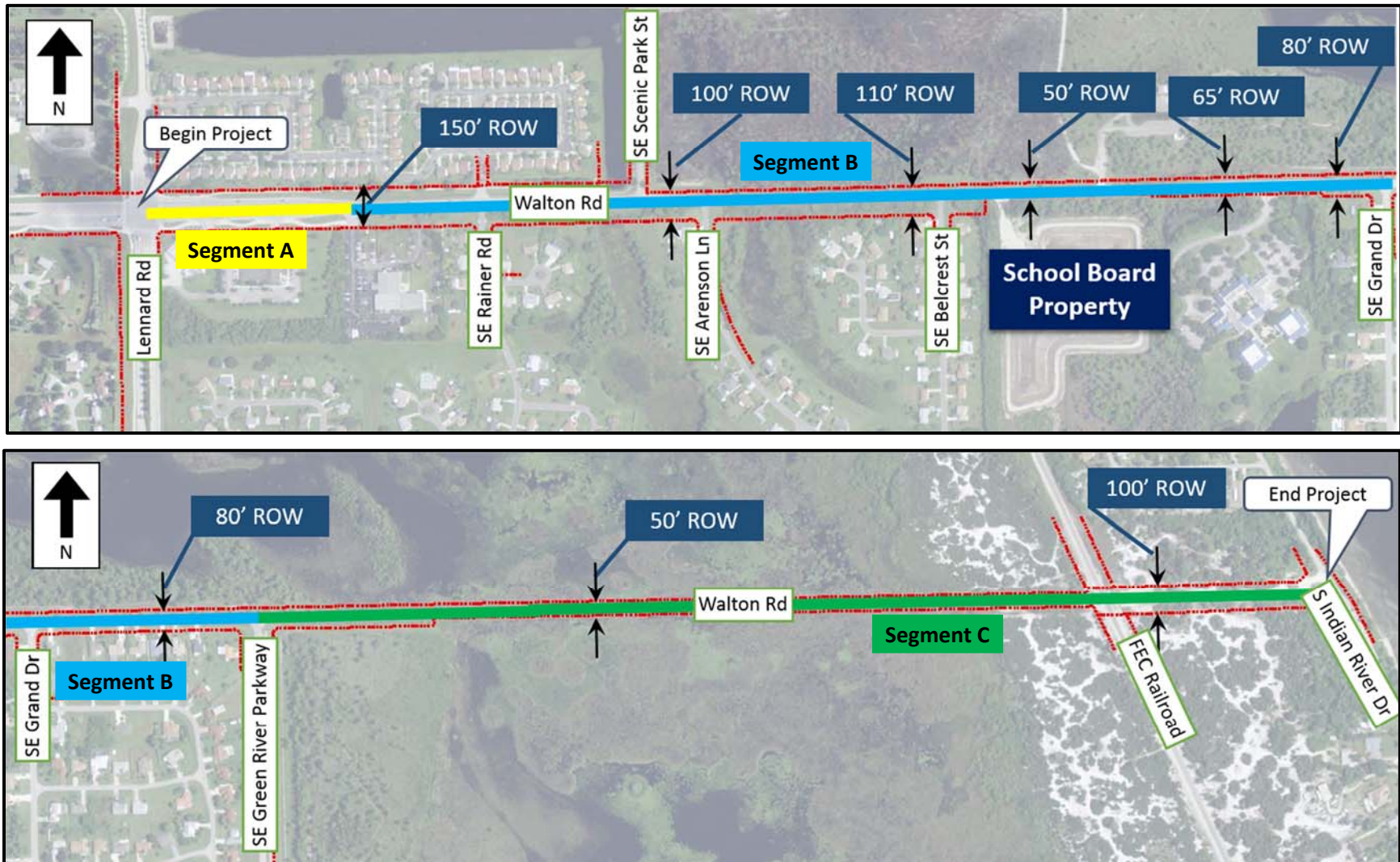


Figure 2.1 Existing Right Of Way



Segment C of Walton Road includes existing guardrail on the north and south side from SE Green River Parkway to the FEC Railroad crossing. The guardrail is damaged in some locations and measured to be approximately nine feet from the edge of pavement. A photo of damaged guard rail can be seen in **Figure 2.2**. The section of damaged guardrail appeared to be marked for maintenance. At some locations the existing guardrail has settled and is located beyond nine feet from the edge of pavement and in standing water. The study area experienced heavy rain prior to the field review.



Figure 2.2 Damaged Guardrail

Information was provided by St. Lucie County Public Works Department (SLCPWD) showing the approximate location of two 48" x 60' RCP and two 72" x 60' CPM culverts east of Green River Parkway. The approximate culvert locations can be seen in the existing conditions map found in **Appendix D** based on a hand sketch provided from St. Lucie County. Additional information provided by SLCPWD verified that a permit was issued by the United States Army Corps of Engineers for the cleaning and restoration of the existing pipes and the work was completed in 1999. These culverts under Walton Road east of SE Green River Parkway were not visually located and could not be verified during the field review. The culverts should be evaluated due to age and environmental conditions.



Figure 2.3 Intersection Lighting

Street lighting in Segment A was located along the north and south side east of Lennard Road and terminated where the roadway transitions from four lanes to two lanes, approximately 800 feet east of the intersection. Intersection lighting is located at the following intersection locations along the study corridor: the northeast corner of SE Scenic Park Street, the southwest corner of SE Grand Drive, and the southwest corner of SE Green River Parkway. A photo of intersection lighting on a single pole can be seen in **Figure 2.3**. Existing overhead utilities are present along the south side of Walton Road and appeared to be within the 20-foot utility easement located adjacent to the existing ROW line shown on the widening plans provided from St. Lucie County. The existing overhead utility lines located on the south side stop east of SE Green River Parkway and continues underground to just west of the FEC Railroad crossing and then continues overhead to Indian River Drive. **Appendix D** provides a detailed map of existing conditions.

No transit stops are currently located within the study corridor, nor do any transit routes pass through the corridor. An exhibit showing nearby transit stops and routes can be found in **Appendix E**.



2.2 Operation

Evaluation of the average vehicle speed was completed based on speed data collected at three locations along the corridor on Thursday March 17, 2016. The posted speed on the study corridor is 45 mph and a posted speed limit sign can be seen in **Figure 2.4**. The results from the data collected east of Lennard Road, as well as east and west of SE Green River Parkway indicate the 85th percentile speed is 43 MPH. The posted speed for Walton Road is 45 MPH. The collected speed data can be found in **Appendix A**.

Field observations confirmed that roadway operating conditions along the corridor and the stop controlled intersections are satisfactory. All stop controlled intersections within the study corridor were observed to operate at an acceptable level of service for vehicles. Several bicyclists were observed sharing the roadway with vehicles.



Figure 2.4 Speed Limit Sign

The current access control is limited to a raised median within the four-lane section east of Lennard Road as well as right and left turn lanes at SE Powderly Place/SE Rainer Road intersection, SE Scenic Park Street and the driveway access to Port St. Lucie Hospital west of SE Grand Drive.

2.3 Safety

A review of crash data received from TPO staff for the most recent 5-year period from 2011-2015 was performed to evaluate safety conditions along the corridor within the study limits.

Table 2.1 Crash Distribution by Year

Year	Number of Crashes
2011	3
2012	2
2013	2
2014	9
2015	12
Total Crashes	28

A total of 28 crashes occurred within the study limits from 2011 to 2015 and is represented in **Table 2.1**. A significant increase in crashes is apparent between year 2013 and 2014. The largest number of crashes was reported for year 2015 with twelve crashes, followed by 2014 with nine crashes. A more detailed crash analysis was completed for 2014 and 2015 due to the large increase of crashes and is presented later in this section.



The crash distribution by location was also analyzed and illustrated in **Figure 2.5**. The intersection with the most amount of crash incidents for the study corridor during the five year period was Lennard Road with nine crashes, followed by Indian River Drive with eight crashes. Seven out of the nine crashes that occurred at Lennard Road were “rear end” crashes, with one crash reported as “other” and one “bicycle” crash. The large number of “rear end” crashes at this intersection could be due to congestion at the intersection signal. Three “rear end”, three “run off” road, and two “other” crashes make up the eight collisions reported for the Indian River Drive intersection. At SE Green River Parkway four out of five of the collisions were “left turn” crashes and one was an “off road” crash. No “pedestrian” collisions were reported for the study corridor. The crash data can be found in **Appendix C**.

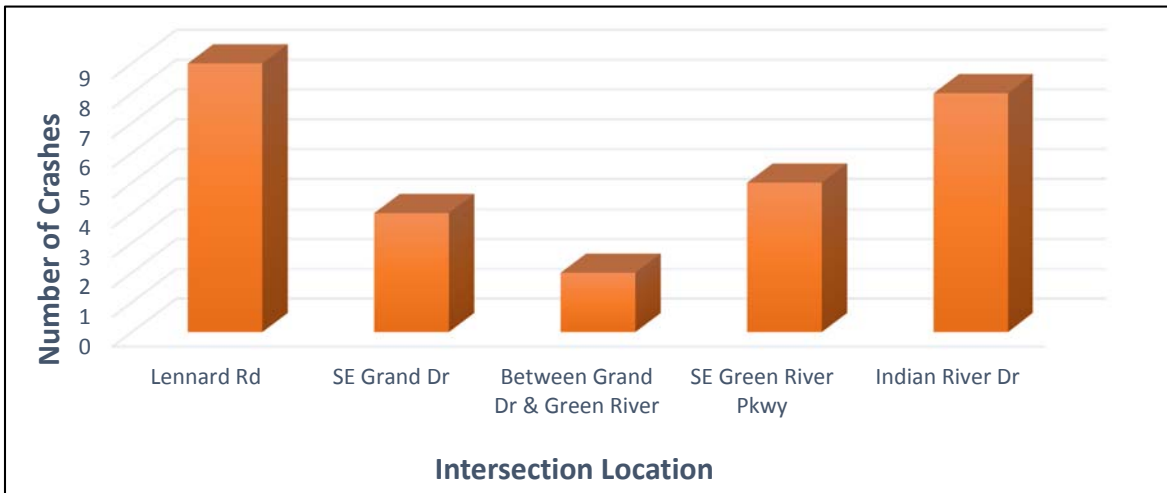


Figure 2.5 Five Year Crash Distribution by Intersection Location

Table 2.2 shows that the majority of crashes occurred during clear conditions. The study corridor does not have a pattern of crashes occurring in wet/rainy conditions.

Table 2.2 Crash Distribution by Weather Condition

Weather Condition	Number of Crashes
Clear	24
Cloudy	1
Rain	2
Fog, Smog, Smoke	1

The majority of crashes occurred in the daylight condition, as shown in **Table 2.3**. Only four crashes were reported in dark, unlit conditions and one dark crash did not report the lighting condition. Including the unknown lighting crash, the percentage of crashes that occurred in dark is 18%.



Table 2.3 Crash Distribution by Lighting Condition

Lighting Condition	Number of Crashes
Daylight	19
Dawn/Dusk	2
Dark - Lighted	2
Dark - Not Lighted	4
Dark - Unknown Lighting	1

Table 2.4 illustrates that Rear End collisions account for the largest percentage of collision type with 12 crashes and 43%. The next major collision type is Off-Road with 6 crashes and 21%. Left Turn crashes was the third most reported collision type with 14% of the crashes within the study corridor. There was one reported bicycle crash that occurred at Lennard Road.

Table 2.4 Crash Distribution by Collision Type

Collision Type	Number of Crashes
Rear End	12
Left Turn	4
Off-Road	6
Bicycle	1
Other	5

Further analysis was completed to evaluate the large increase of crashes from 2013 to 2014 and from 2014 to 2015. **Table 2.5** and **Table 2.6** illustrate the detailed analysis completed for the year 2014 by crash location and crash type.

Table 2.5 Location Distribution Year 2014

Intersection Location	Number of Crashes
Lennard Road	3
SE Grand Drive	1
Between Grand Drive & Green River	0
SE Green River Pkwy	2
Indian River Drive	3

Table 2.6 Crash Type for Year 2014

Collision Type	Number of Crashes
Rear End	3
Left Turn	2
Off Road	3
Bicycle	0
Other	1



All the crashes for 2014 were analyzed by location and crash type in order to determine if a pattern exists to explain the jump from two crashes in 2013 to nine crashes in 2014. Lennard Road and Indian River Drive were reported to have three crashes each. The most common crash type for 2014 was rear end crashes and 3 off road crashes. The three crashes that occurred at Lennard Road were two rear end crashes and one other crash. The three crashes that occurred at Indian River Drive consisted of one rear end crash and two off road crashes.

Further detailed crash analysis was also completed for year 2015 and can be seen in **Table 2.7** and **Table 2.8**.

Table 2.7 Location Distribution for Year 2015

Intersection Location	Number of Crashes
Lennard Road	5
SE Grand Drive	0
Between Grand Drive & Green River	2
SE Green River Pkwy	1
Indian River Drive	4

Table 2.8 Crash Type for Year 2015

Collision Type	Number of Crashes
Rear End	7
Left Turn	0
Off Road	2
Bicycle	0
Other	3

It can be seen in **Table 2.7** that for the year 2015 five crashes occurred at the intersection of Lennard Road. Further analysis revealed that all five of those crashes are rear end crashes. Out of the four crashes that occurred at Indian River Drive, two were rear end crashes and two were other crashes.

From conducting additional analysis for 2014 and 2015 it can be concluded that more crashes occurred on Walton Road at the intersections of Lennard Road and Indian River Drive. These rear end crashes could have occurred more frequently in 2014 and 2015 due to multiple reasons including driver error/distraction and/or congestion.

2.4 Traffic

Existing traffic volume data was collected on March 17, 2016 at three locations along the corridor. The average daily traffic (ADT) for the location east of Lennard Road is 9,997 vehicles per day. The location west of Green River Parkway has an ADT of 8,625 vehicles per day and east of SE Green River Parkway the ADT is 6,285. The daily traffic volumes from the count data appear to be consistent when compared to the annual average daily traffic (AADT) shown in the St. Lucie County TPO Traffic Counts and Level of Service Report Fall 2015. In this report, the AADT from Lennard Road to SE Green River Parkway is 9,600 and the ADT from SE Green River Parkway to Indian River Drive is



5,700. Based on the 2016 ADT, the roadway is operating at an acceptable level of service (LOS) C for a two-lane undivided urban minor arterial.

The 2040 future volumes for Walton Road are from the TCRPM 4 model used for the adopted 2040 Cost Feasible alternative analysis for the Go2040 LRTP update. These future volumes and the St. Lucie County TPO 2015 AADT are represented in **Table 2.9**.

Table 2.9 Traffic Volumes and LOS for Walton Road

Walton Road Section	2015 AADT	2040 Volumes	Future LOS
Lennard Road to Green River Pkwy	9,600	12,480	C
Green River Pkwy to Indian River Drive	5,700	9,600	C

A preliminary LOS analysis was completed for the expected future year volumes of 2040 using the Florida Department of Transportation Generalized Level of Service tables from the most recent (2013) Quality/Level of Service Handbook. For an urban undivided two lane roadway section with a speed greater than 40 mph, the LOS AADT volume threshold is 15,120. This volume accounts for the roadway being a non-state signaled roadway. The predicted future 2040 volume meets the threshold for a two-lane undivided roadway. Therefore, the need for capacity improvements is not expected.



2.5 Environmental Conditions

A review of existing environmental conditions for the study corridor was completed. According to the U.S. Fish and Wildlife Service National Wetlands Inventory database there are wetlands adjacent to the study corridor. The wetland map can be seen in **Figure 2.6**.

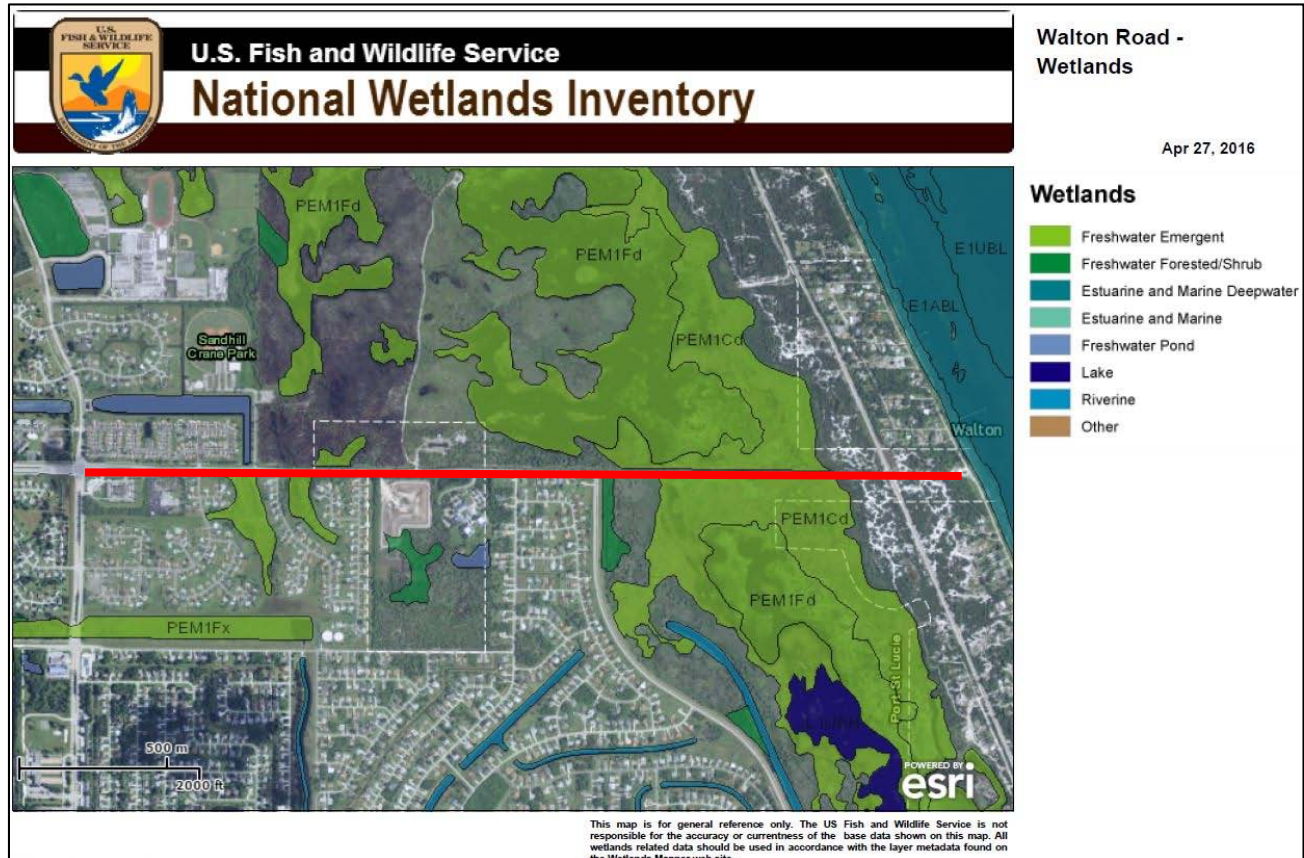


Figure 2.6 Wetland Map

Generally the area beyond the roadside shoulders are wet and contain pools of water. A photo of the wetlands observed between SE Arenson Lane and SE Grand Drive is shown in **Figure 2.7**.

A preliminary review of threatened and endangered species within the study corridor revealed a total of 26 species and 35 migratory birds using the Information for Planning and Conservation (IPaC) tool from the U.S. Fish & Wildlife Service website. Some notable endangered species that have a potential to be located throughout the study area include the Everglade Snail Kite, Florida Scrub Jay, Wood Stork, and the Eastern Indigo Snake. A copy of the full report can be found in **Appendix A**.



Figure 2.7 Roadside Wetland



2.6 Deficiencies

During the office and field review the study corridor was analyzed to determine multimodal deficiencies including missing bicycle and pedestrian facilities, lighting, paved shoulders and signage. The following deficiencies were noted:

- There are no paved shoulders on the two-lane section east of Lennard Road to Indian River Drive as shown in **Figure 2.9**.
- The only pedestrian crossings within the corridor are located at Lennard Road.
- There is no existing sidewalk on the north side of Walton Road east of SE Scenic Park Street and no sidewalk on the south side beyond the existing off-road shared use path ending west of SE Rainer Road.
- There is no street lighting along Walton Road on the two-lane section east of Lennard Road to Indian River Drive except at the SE Scenic Park Street and SE Green River Parkway intersections.



Figure 2.9 Walton Road Two-Lane Section



3.0 Alternatives Evaluation

Existing multimodal facilities consisting of on-street bike lanes, off road shared use path and sidewalk are located within Segment A; therefore, no alternatives are presented for Segment A in this study. Alternatives for adding multimodal improvements in Segment B and C were evaluated based on a planning level analysis with respect to available right of way, connectivity to existing bicycle and pedestrian facilities, environmental and social-cultural factors as well as feasibility of implementation. Increasing mobility and improving safety for both bicycles and pedestrians by enhancing or expanding existing facilities was part of the evaluation process. There are no existing multimodal facilities on Indian River Drive. Multimodal improvements to Indian River Drive would require a separate study.

As stated in **Section 2.4**, the existing 2016 traffic volumes on Walton Road show that the two-lane section is operating at an acceptable LOS and projected 2040 traffic volumes indicate widening of the roadway is not expected to be needed to meet future capacity requirements. Therefore, roadway capacity improvements were not evaluated for this study. All the alternatives presented in this feasibility study would require a design variance from St. Lucie County to reduce roadway lane widths to eleven feet. Alternatives presented in this section are summarized in **Table 3.1**.

Table 3.1 Summary of Alternatives

Segment	Alternatives
Segment A	Maintain existing multimodal features
Segment B	<ol style="list-style-type: none"> 1. Bike lanes, sidewalk, multiuse path 2. Curb and gutter – bike lanes, sidewalk, multiuse path
Segment C	<ol style="list-style-type: none"> 1. Add bike lanes & sidewalk 2. Roadway reconstruction with curb and gutter – add bike lanes & sidewalk 3. No roadway improvements – add raised shared use boardwalk 4. Bridge roadway – add sidewalk and paved shoulders

Segment B has two alternatives; Alternative B1 includes four-foot paved shoulder bike lanes, a sidewalk on the north side and a shared use path on the south side, Alternative B2 reconstructs the roadway with curb and gutter and includes four-foot paved shoulder bike lanes, a sidewalk on the north side and a shared use path on the south side of Walton Road.

Four alternatives are presented for Segment C. Alternative C1 includes adding fill for paved bike lanes and sidewalk north along Walton Road with a roadside swale south of Walton Road for drainage. This alternative would require additional right of way through the Savannas Preserve State Park depending on elevation changes and not including temporary construction easements. Alternative C2 recommends reconstructing the roadway, adding curb and gutter, a pond and paved shoulder bike lanes, with a sidewalk along the north side of Walton Road. The roadway would need to be elevated approximately four feet. This would provide enough hydraulic grade for runoff to be



routed to a proposed pond at the vacant school board parcel located to the east of Belcrest St. A drainage report and analysis would need to be completed during design that will support the permit requirements. In order not to require additional right of way, this alternative initially utilized Mechanically Stabilized Earth (MSE) wall to eliminate side slopes and additional right of way. Constructing MSE wall through Savannas Preserve State Park is not considered feasible since this concept limits wildlife crossings and does not confirm to the aesthetics of the adjacent land use. Therefore, Alternative C2 was re-evaluated with roadside slopes that encroach outside of the existing ROW into the Savannas Preserve State Park. Permitting and mitigation would be required with federal & state agencies. Some mitigation credits might be earned through the improvements presented in this alternative since stormwater runoff would be conveyed to a proposed pond for water quality in lieu of direct discharge to the park. Coordination with permitting agencies is required for consideration of this possibility. Alternative C3 is a lower cost alternative for Segment C that proposes no roadway improvements and a separate elevated shared use 10-foot boardwalk structure extending on the north side along Walton Road through Savannas Preserve State Park and continuing with a six-foot sidewalk west of the FEC railroad crossing to Indian River Drive. The elevated boardwalk may require additional ROW, depending on the design and placement of guardrail and the deflection distance to the boardwalk, within the 50-foot ROW section that traverses through the State Park. Depending on the guardrail replacement, additional ROW might not be needed.

A fourth alternative was added to Segment C due to comments from the June 1, 2016 TPO Board meeting. Alternative C4 is a bridge alternative that proposes building a new bridge on Walton Road east of Green River Parkway spanning approximately 3,000 feet over the Savannas Preserve State Park to west of the FEC crossing. The bridge design evaluated is concrete pre-stressed beams (Florida I Beams) with a concrete deck and supported in pile bents. The typical section of the bridge would include 11-foot travel lanes, 8-foot paved shoulders, type F barriers on both sides, a 6-foot sidewalk and pedestrian railing. Drainage and a pond would also need to be considered for this alternative in complying with runoff water quality standards. All four alternatives for Segment C would require pedestrian crossing improvements at the FEC railroad crossing.

A pond for water quality treatment will be needed if curb and gutter in Alternative B2, C2 and/or C4 is placed along Walton Road. A pond location option could be the vacant parcel located east of Belcrest Street which is owned by St. Lucie County School Board. Substantial financial savings is possible if the School Board is willing to donate ROW for the pond.

Furthermore, all alternatives in Segment C would require additional ROW and permitting with federal and state agencies during design for construction. In the past, Stanley Consultants has experienced a time frame of six to eight months for permit modifications or new permit applications. The cost of the permit is dependent on the type of permit required (modification or new) and the improvements being competed. If mitigation is required, it is recommended to use a mitigation bank. Moreover, it is recommended that pre-application meetings be held with South Florida Water Management District (SFWMD) and the US Army Corps (land owners) in order to ensure that the alternative will avoid or minimize impacts and discuss and possible mitigation credits for improving the area's hydrology if culverts are replaced.



3.1 Bike Lanes and Paved Shoulders

Adding on-street bike lanes (paved shoulders) to the two-lane section in Segments B and C was evaluated based on increasing safety within the corridor and increasing bicycle mobility to existing surrounding bike paths. A four foot bike lane is the minimum functional width for a roadway without curb and gutter and flush shoulders per Chapter 9 of the Florida Green Book. Paved shoulders, four feet or wider, may be marked as bicycle lanes and considered to be bicycle facilities.

Bicycle counts were taken during the AM and PM peak period on a weekday and also during the midday period on a weekend to evaluate existing bicycle usage along the corridor. The count data at the Lennard Road intersection indicated a low number of riders using the roadway but some bicyclists were observed riding on the existing off road shared use path. Data from the counts at the Green River Parkway intersection also showed a low number of riders overall with the largest number of bicyclists south of the intersection utilizing the shared use path. However, some bicyclists were observed riding east of Green River Parkway on Walton Road towards Indian River Drive reassessing the need of bike lanes for safety along this portion of roadway. The collected bicycle count data can be found in **Appendix A**.

There are two sections within the study corridor with existing ROW widths of 50 feet. A photo of the two-lane section with an existing 50 foot ROW width east of SE Green River Parkway is shown in **Figure 3.1**. One section in Segment B is located east of SE Belcrest Street spanning the current vacant school board parcel and the other section in Segment C is located from east of SE Green River Parkway to west of the FEC Railroad. Adding paved shoulder bike lanes in each direction is included for both Segment B alternatives and in Segment C for the roadway widening (Alternative C1), roadway reconstruction (Alternative C2), and bridge roadway (Alternative C4) alternatives.



Figure 3.1 Two Lane Section from Segment C

The cost for bike lane improvements was estimated using the FDOT Long Range Estimation System for milling and resurfacing a two-lane rural road with five-foot paved shoulders and will be included for the overall cost estimate for each alternative presented later in this section. A typical section diagram showing how bicycle lanes can be accommodated within the existing ROW width is also presented later in this section.



3.2 Sidewalks

Adding missing sidewalks and connecting residential areas to the Savannas Preserve State Park as well as increasing pedestrian access, safety and enhancing mobility was evaluated for the entire corridor.

Pedestrian counts were taken during the AM and PM peak period on a weekday and also during the midday period on a weekend to evaluate existing pedestrian usage along the corridor. The count data at the Lennard Road intersection showed no pedestrians using the sidewalk on the north side of Walton Road but some pedestrians were observed walking on the shared use path. During the field review one pedestrian was observed walking along the south side of Walton Road between SE Arenson Lane and SE Scenic Park Street as shown in **Figure 3.2**.



Figure 3.2 Pedestrian Walking Along Walton Road

Data from the count at the Green River Parkway intersection also revealed a low number of pedestrians overall and all observed pedestrians were using the existing paved trail on the east side of SE Green River Parkway south of Walton Road. The collected pedestrian count data can be found in **Appendix A**.

Adding a six-foot wide sidewalk on the north side of Walton Road in Segment B between SE Scenic Park Street and SE Green River Parkway is included for both alternatives. Adding six-foot wide sidewalk on the north side of Walton Road from SE Green River Parkway to Indian River Drive in Segment C is included for Alternatives C1 and C2. There is a possibility that portions of the proposed sidewalk will traverse existing wetland areas requiring fill, gravity walls and lateral offsets to minimize impacts to environmentally sensitive areas. It is recommended that the quality of delineated wetland areas be evaluated during the design and permitting phases to make a determination of potential impacts and costs. A six-foot sidewalk could be provided for the Walton Road Bridge Alternative C4 on the south side of Walton Road.

The estimated cost associated with only the sidewalk was based on projects listed in the latest St. Lucie TPO TIP and was estimated to be approximately \$390,800 per mile. Cost estimates for the sidewalk for each section are shown in **Table 3.2**. A typical section diagram showing how sidewalks can be accommodated within the existing ROW width is presented in **Section 3.5**.

Table 3.2 Sidewalk Estimated Cost per Study Section

Segment	Sidewalk Cost
Segment A	N/A
Segment B (Alternatives B1 and B2)	\$281,000
Segment C (Alternatives C1 and C2)	\$313,000
Segment C (Alternative C3)	\$98,000



3.3 Shared Use Path

Continuing the existing off-road shared use path on the south side of Walton Road in Segment B to the shared use path east of Green River Parkway was evaluated based on increasing both bicycle and pedestrian mobility for all users. Based on the Bicycle/Pedestrian Corridor Study Area of Interest Analysis completed by the St. Lucie TPO in November 2011, continuing the existing off-road path east of Lennard Road to the existing Green River Trail was included as part of the recommended preferred alternative. In addition to recommendations from previously completed studies, counts taken at both the Lennard Road and Green River Parkway intersections, documented that there is some current usage of the existing off road path and paved trail by both bicyclists and pedestrians. The collected bicycle and pedestrian count data can be found in **Appendix A**.

Continuing the meandering 12-foot wide off-road path, east of Lennard Drive, between the existing edge of pavement and 20-foot wide utility easement adjacent to the south ROW line is included in Segment B. Coordination with utility owners will be required. However, there would be wetland impacts within Segment B due to clearing and grubbing and fill material necessary to construct the road. To minimize wetland impacts, an elevated boardwalk style structure with a pedestrian railing at a minimum of 10 feet wide would have less impacts than an at-grade paved path. Moreover, in order to reduce wetland impacts in Segment C, Alternative 3C recommends adding a 10 foot wide elevated boardwalk style structure on the north side of Walton Road through Savannas Preserve State Park and not implementing any improvements to the roadway. There are no shared use path improvements presented in Alternative C1 and C4. The typical section of Alternative C2 was evaluated with a shared use path and no sidewalk in order to provide better level of service for pedestrians but would require even more ROW. Therefore, Alternative C2 does not provide a shared use path.

The segment east of SE Belcrest Street adjacent to the parcel owned by the St. Lucie County School District has an existing ROW width of 50 feet. Continuation of the 12' wide off road shared use path along the south side in addition to on-street bikes lanes and sidewalk on the north side is not feasible within the existing ROW. Based on coordination with St. Lucie Public Schools, the property owned by the school district could be donated for ROW to add off-road shared use paths or sidewalks.

The estimated cost associated with only the shared use path improvements is shown in **Table 3.3**. The estimated costs were based on \$400,000 per mile of path. For the elevated boardwalk in Segment C a cost estimate of \$1,200,000 per mile was used based on coordination with St. Lucie County Environmental Resources regarding the existing boardwalk in Savannas Preserve State Park. A typical section showing the shared use path improvements is presented in **Section 3.5**.

Table 3.3 Shared Use Path Estimated Cost per Study Section

Segment	Shared Use Path Cost
Segment A	N/A
Segment B (Alternatives 1B and 2B)	\$368,000
Segment C (Alternative 3C)	\$600,000



3.4 Pedestrian Crossings

Pedestrian crossings at the SE Belcrest Street and SE Green River Parkway intersections are recommended for better connectivity with residential areas along the south side of Walton Road to Sandhill Crane Park and Savannas Preserve State Park located on the north side of Walton Road. **Figure 3.3** shows an example of a proposed pedestrian crossing using Florida Green Book standards.

Cross walk markings with a minimum of a Pedestrian Warning Sign (W11-2) and diagonal downward pointing arrow (W16-7P) plaque together with a roadside flashing beacon assembly or rectangular rapid flashing beacon would enhance pedestrian safety by improving the crosswalk visibility. Additionally, a Pedestrian Warning Sign (W11-2) and supplemental ahead (W16-15P) plaque are also recommended in advance of all cross walk markings as an added safety benefit. It is recommended that these cross walk improvements be made at all cross street intersections with Walton Road and two north-south crossings, east of SE Belcrest Street and east of SE Green River Parkway. The location of these recommended improvements can be found in **Appendix F**.



Figure 3.3 Example of Pedestrian Crossing on Green River Parkway

3.5 Alternative Typical Sections

There are several alternatives presented in this study for the Walton Road corridor from Lennard Road to Indian River Drive. Due to the existing multimodal conditions, no multimodal improvements for Segment A, were recommended.

The typical section for Alternative B1, of Segment B, can be seen in **Figure 3.4**. In Alternative B1, bicycle lanes are proposed for both travel directions on four-foot paved shoulders with sufficient pavement markings and signs. The shared use path is extended from 800 feet east of Lennard Road on the south side of Walton Road to connect with the shared use path east of Green River Parkway providing off road connectivity for the East Coast Greenway. The sidewalk on the north side of Walton Road is extended from SE Scenic Park Street to Green River Parkway. The minimum ROW requirement for this typical section is expected to be 65 feet with 11-foot roadway widths, four-foot bike lanes, five to six-foot sidewalk, and 10 to 12-foot shared use path.

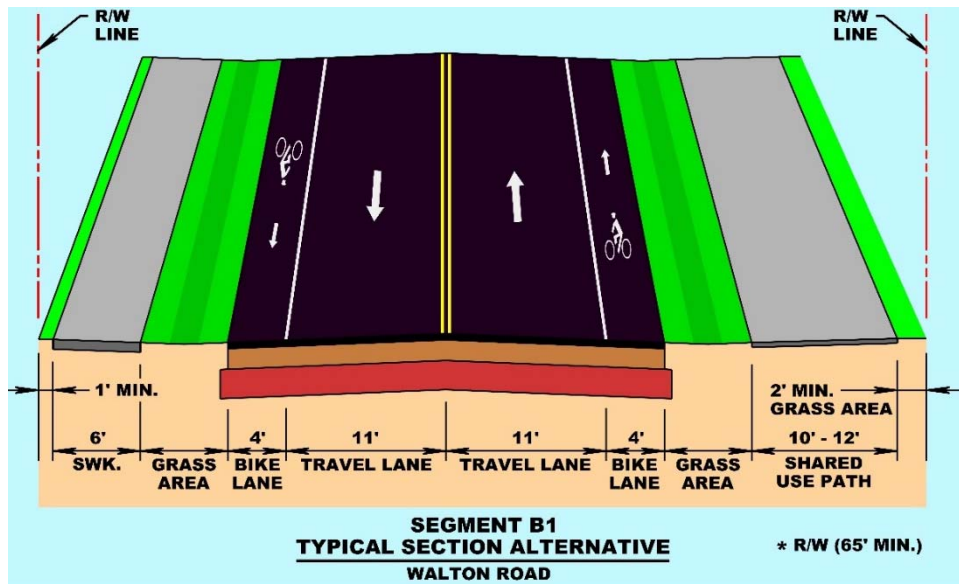


Figure 3.4 Alternative B 1 Typical Section

A second alternative for Segment B is Alternative B2, which provides the same multimodal improvements of Alternative B1 but includes the addition of curb and gutter along both sides of Walton Road. Currently, there is curb and gutter along the four lane section of Walton Road east of Lennard Road, and a matching curb and gutter typical section could also be constructed in Segment B.

The typical section for the Alternative C1 can be seen in **Figure 3.5**. Alternative C1 proposes bicycle lanes for both travel directions on four-foot paved shoulders with sufficient pavement markings and signs, sidewalk on the north side of Walton Road to extend to Indian River Drive on gravity wall with a pedestrian railing and separated from the bike lane with a two-foot shoulder and three-foot guardrail, and a swale and guardrail on the south side. Sidewalk could not be provided on the south side of Walton Road due to ROW restrictions.

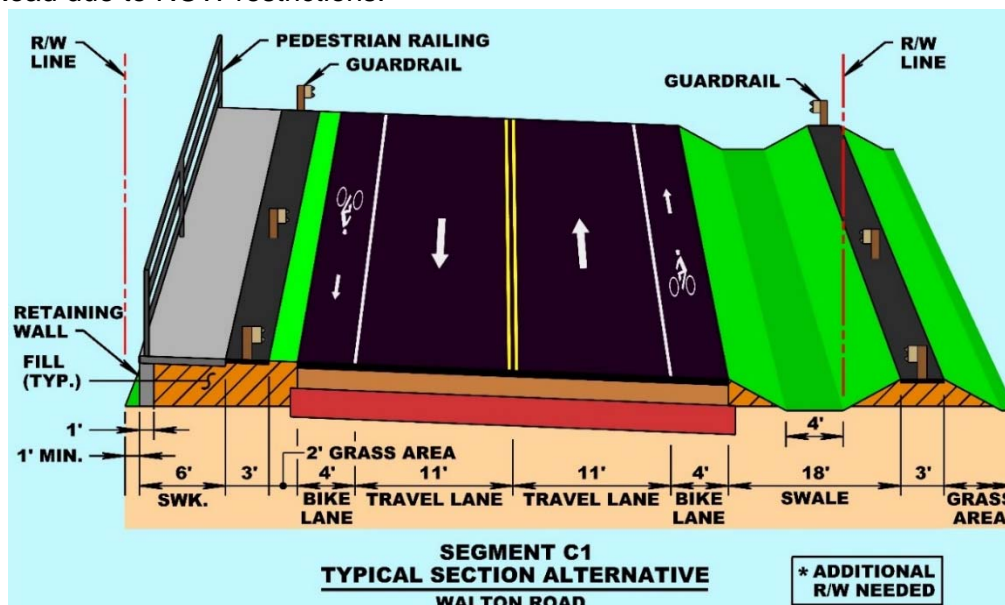


Figure 3.5 Alternative C1 Typical Section



In order to reduce impacts to surrounding wetlands and the State Park, the typical section recommends 11-foot roadway widths, four-foot bike lanes and five to six-foot sidewalk. Preliminary calculations indicate 0.60 acre feet of water retention is needed to meet water quality criteria. This can be accomplished within a roadside swale. Florida Greenbook requires the bottom width of a new swale to be four feet. A drainage analysis and report are recommended during the design phase to support the permitting requirements.

Due to the additional ROW needed to accommodate the swale in Alternative C1, another typical section for Segment C was evaluated which includes curb and gutter. Alternative C2 can be seen in **Figure 3.6**. Alternative C2 proposes roadway reconstruction and elevating the roadway by approximately four feet in order to route runoff to a proposed pond located west of Segment C at the St. Lucie School Board's vacant parcel.

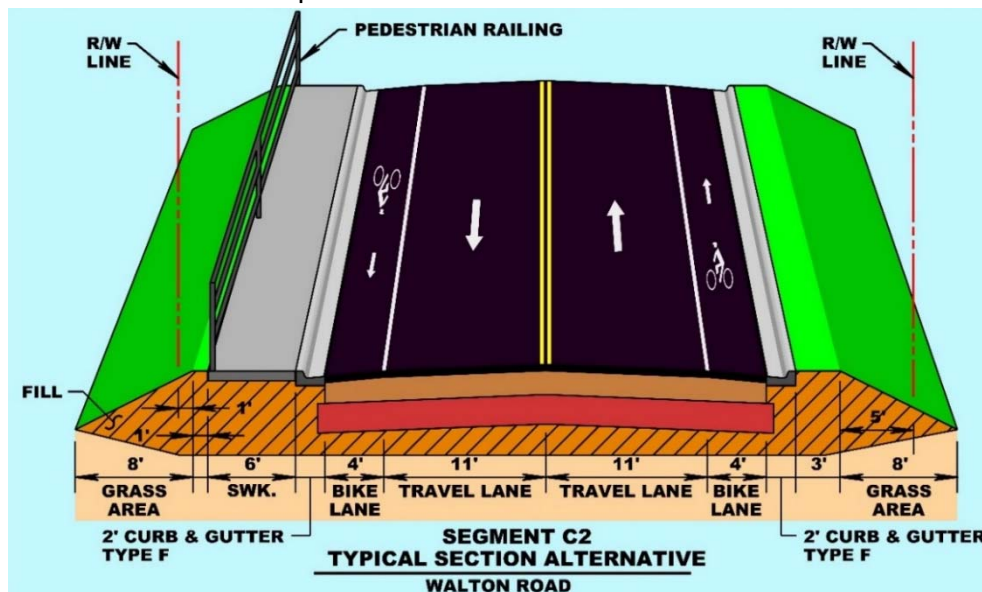


Figure 3.6 Alternative C2 Typical Section

In order to reduce ROW impacts, Alternative C2 was first evaluated with MSE wall and roadway barrier but this is not considered feasible because it does not promote wildlife crossings or recreational use. The alternative was then re-evaluated with roadside slopes that will encroach beyond the existing ROW.

For the typical section in Alternative C2, the sidewalk proposed for this alternative is located adjacent to the curb and gutter which improves pedestrian safety and pedestrian Level of Service (LOS) from the existing conditions.

The roadway typical section for Alternative C2 was further evaluated to see if pedestrian LOS could be further improved by providing multimodal facilities separated from the roadway travel ways. It was determined that by reducing the shoulder width to 2 ft on both sides, removing the 6-foot sidewalk and providing a 10-foot multi-use path with the required minimum of a 3-ft grass section, the typical section would be 4 ft wider then providing 4-ft paved bike lanes and 6ft sidewalk. Therefore, the separated multimodal facilities were remove from the alternative.



A third alternative was evaluated for Segment C that has no roadway improvements but provides an elevated shared use boardwalk structure. Alternative C3 can be seen in **Figure 3.7**. This alternative is a lower cost alternative and depending on the guardrail re-placement, additional ROW might not be needed.

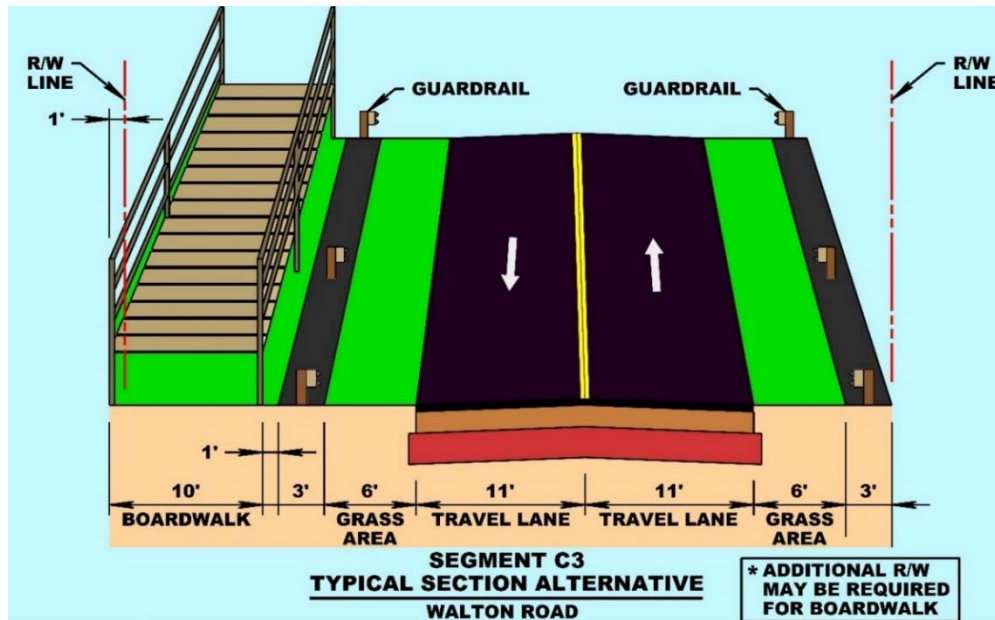


Figure 3.7 Recommended Alternative for Segment C Typical Section

A fourth alternative was added to Segment C due to comments from the June 1, 2016 TPO Board meeting. Alternative C4 is a bridge alternative that proposes building a new bridge on Walton Road east of Green River Parkway spanning approximately 3,000 feet over the Savannas Preserve State Park to west of the FEC crossing. The bridge design evaluated is a concrete pre-stressed beams (Florida I Beams) with a concrete deck and supported in pile bents. The fifty-foot typical section of the bridge would include 11-foot travel lanes, 8-foot paved shoulders, type F barriers on both sides, a 6-foot sidewalk and pedestrian railing. Protected bike lanes were considered but not included in the alternatives because the existing bike lanes on Walton Road are unprotected and the additional cost associated. A planning level cost estimate of the Bridge alternative is roughly \$44 million.

3.6 Total Alternative Costs

As stated in Section 3.1, the cost for adding bicycle lanes was estimated using the FDOT Long Range Estimation System for milling and resurfacing a two-lane rural road with five-foot paved shoulders. This estimated cost is provided by the FDOT per mile of roadway and is included in Appendix F. The estimated cost for milling and resurfacing the two-lane roadway as well as the addition for bicycle lanes for each study segment are provided in Table 3.4.

Table 3.4 Estimated Roadway and Bike Lane Cost

Segment	Milling and Resurfacing with Bike Lanes
Segment A	N/A
Segment B (Alternatives B1 and B2)	\$392,000
Segment C (Alternative C1)	\$341,000



Alternative C2 proposes adding bike lanes with a reconstruction of the current roadway with curb and gutter and elevating the roadway four feet. A preliminary planning level cost estimate of these improvements is provided in **Table 3.5** below.

Table 3.5 Alternative C2 Cost Estimate for Reconstruction

Description	Unit	Price	Approximate Cost
Embankment and Fill (CY)	18,245.00	\$ 20.90	\$ 381,320.50
Roadway with 5' paved shoulders (mile) - curb & gutter, inlets	0.8	\$ 4,200,000.00	\$ 3,360,000.00

The total of each roadway and multimodal improvement estimated cost per each alternative is provided in **Table 3.6**. Contingency, maintenance of traffic, preliminary engineering and design and construction engineering and inspection was approximated at 10% construction cost. The cost estimation for Segment C includes an estimated \$145,000 FEC railroad crossing improvements cost. If yearly maintenance for Alternative C3 is assumed at \$1 per square foot, the boardwalk alternative would incur a \$72,000 yearly maintenance cost. Assuming that the boardwalk is adequately maintained, the expected life cycle of the boardwalk is 16 years.

Mitigation, permitting and right of way costs are not factored into the total alternative cost estimate presented in **Table 3.6**.

Table 3.6 Total Alternative Cost Estimate

Segment	Alternative	Description	Approximate Cost
Segment A (Lennard to 800' E of Lennard)	Maintain existing facilities	Maintain existing multimodal features	N/A
Segment B (800' E of Lennard to Green River Parkway)	B1	Bike lanes, sidewalk, multiuse path	\$ 1,980,000
	B2	Curb and gutter – bike lanes, sidewalk, multiuse path	\$ 6,980,000
Segment C (Green River Parkway to Indian River Drive)	C1	Add bike lanes and sidewalk	\$ 3,100,000
	C2	Roadway reconstruction with curb and gutter- add bike lanes & sidewalk	\$ 6,500,000
	C3	No roadway improvements- add raised boardwalk	\$ 1,310,000
	C4	Bridge roadway – add sidewalk and paved shoulders	\$43,950,000

3.7 Other Multimodal Alternatives

Other multimodal alternatives were considered for this feasibility study due to the expressed intent of the St. Lucie TPO Board to improve connectivity of the kayak launch located south of the study area to the area of Savannas Preserve State Park north of Walton Road. Currently Walton Road divides the multimodal trail of Savannas Preserve State Park. Two options were evaluated and presented in the following sub sections.



3.7.1 Crossing Under Walton Road Option

Currently there are four culverts located under Walton Road in Segment C. It could be considered to replace one these culverts with an arch culvert large enough to accommodate multimodal users in the Savannas to gain access and connectivity within the park. The culvert would need to be placed on adequate foundations, roadway embankment will be needed for the roadway to pass over the culvert, and MSE wall will be needed to retain the embankment in minimal ROW conditions and have a five-foot minimum required vertical clearance between the lowest member and the design high water table elevation. Other considerations for the culvert include enough natural daylight passing through the 50 foot long section for safety and user encouragement. The design of the kayak crossing should be coordinated with St. Lucie County's Environmental Resources Department in order to provide an animal crossing. A rough estimate for this option, assuming roadway construction cost is absorbed in Segment C cost estimation, is approximately \$900,000. If Alternative C3 is moved forward, the cost of roadway reconstruction (base material and asphalt) would need to be included in the Kayak Crossing cost estimation. This would raise the cost estimation for alternative 3C to \$1,750,000.

3.7.2 Crossing Over Walton Road Option

Another option that could be considered in order to improve the connectivity of Savannas Preserve State Park for multimodal water users is a kayak/canoe portage where users would carry their non-motorized watercraft over Walton Road. Adequate ingress and egress structures or embankment would be needed for all users to be able to access the portage and a mid-block pedestrian crosswalk with a button control signal would need to be provided. A rough estimate for this option is approximately \$115,000.

3.8 Other Project Considerations

Discussion with the St. Lucie County Environmental Resources Department found that they are currently applying for multimodal improvements for Walton Road from Green River Parkway west to the entrance of Savannas Preserve State Park through a Sun Trail funding application. Coordination should be completed with St. Lucie County in order to efficiently allocate multimodal funding for the corridor without duplicated effort. A representative at St. Lucie County also mentioned a new trail project effort to re-route the east coast Greenway Trail through the Savannas Preserve State Park which would require DEP to provide easement for a boardwalk trail structure. A telephone record with St. Lucie County can be found in Appendix F.

Any alternative chosen to move forward into design and construction will require permitting. It is recommended that pre-application meetings be held with South Florida Water Management District and DEP to lessen challenges and expedite the permit process. In previous design projects completed by Stanley Consultants the permit process typically takes between six months to one year. The cost of the permit varies with amount of right of way impacted, the type of impact and if a new or modification permit is required. The estimated cost for permitting is approximately \$3,000 but varies per agency. If a permit has more than 0.5 acres of wetlands impacted mitigation will need to occur. Mitigation costs could not be accurately estimated for this study due to the amount of variables factored into the cost. Mitigation costs are provided from the Florida Department of Environmental Protections and vary on quality of wetlands, amount, type of impact, and if the impacts have to be mitigated at a rate greater than one to one.



If the existing culverts located on Walton Road through the Savannas Preserve State Park are determined to need replacement through the findings of a separate hydrology and hydraulic study, a preliminary cost estimation was completed in this study. The findings of the preliminary cost estimation are summarized in **Table 3.7**.

Table 3.7 Culvert Replacement Cost Estimate

Description	Unit	Total Cost
48" x 60' RCP pipe & Endwall concrete and steel	2	\$ 90,000
72" x 60' CMP & Endwall concrete and steel	2	\$ 141,000
Total (including contingency, preliminary engineering & CEI)		\$ 304,000



4.0 Conclusions and Recommendations

4.1 Conclusions

Based on data collected and information received during the office and field reviews, an analysis of the Walton Road corridor existing conditions showed there are deficiencies with bicycle, pedestrian and transit facilities as well as street lighting. For the purpose of this report the corridor was divided into three segments as seen in **Table 4.1**.

Table 4.1 Study Corridor Segment Description

Segment Name	Description
Segment A	Lennard Road to 800' east of Lennard Road
Segment B	800' east of Lennard Road to Green River Parkway
Segment C	Green River Parkway to Indian River Drive

Deficiencies found within the study corridor include: no bike lanes or paved shoulders in Segment B and C, sidewalk missing in Segment B and C, no pedestrian access to Savannahs Preserve State Park in Segment B, and a gap in the existing off-road shared use path on the south side of Walton Road to Green River Parkway for the East Coast Greenway. The existing roadway physical and operating conditions were documented. Walton Road has excess capacity based on existing and projected traffic volumes, and an 85th percentile speed of 43 MPH which is below the posted speed of 45 mph. The evaluation found that there is not sufficient ROW width at some locations for incorporating the proposed multimodal improvements. In addition, the property owned by the school board may be needed for stormwater management and temporary construction easements for Alternative C. All alternatives proposed for Segment C would require ROW acquisition, with Alternative 3C requiring the least amount or possibly none.

Field observations confirmed that the roadway operating conditions along the corridor and the stop controlled intersections are satisfactory. According to the field review, all intersections within the study corridor appeared to operate at an acceptable level of service for vehicles. Several bicyclist and pedestrians were observed sharing the roadway. A review of crash data received from TPO staff for the most recent 5-year period from 2011-2015, found a total of 28 crashes occurred within the study limits. No "pedestrian" collisions were reported, and one "bicycle" crash occurred within the study corridor at the intersection of Lennard Road. However, there is a need for multimodal improvements within the corridor to enhance mobility, connect existing shared use paths, and provide pedestrian access to Savannahs Preserve State Park.

Based on the evaluation of alternatives, adding on-street bike lanes, sidewalk on the north side and completing the missing link of off-road shared use path between Lennard Road and SE Green River Parkway are proposed for Segment B. Both on-street bike lanes and sidewalk on the north side are also possible based on the existing 50 to 100 feet ROW width between SE Green River Parkway and Indian River Drive within Segment C but ROW, permitting and stormwater drainage costs significantly increase the total cost of this alternative. These improvements would provide safety benefits to both bicyclists and pedestrians as well as potential connections to future trails.



4.2 Recommendations

There are two alternatives for Segment B and four alternatives for Segment C presented in this study for the Walton Road corridor from Lennard Road to Indian River Drive. A summary of these alternatives are summarized in **Table 4.2** below. The alternatives for Segment C include the \$145,000 FEC crossing, \$3,000 permit fee and the under Walton Road kayak option.

Table 4.2 Study Alternatives

Segment	Alternative	Total Cost
Segment A	Maintain existing multimodal features	None
Segment B	B1. Bike lanes, sidewalk, multiuse path	\$ 1,980,000
	B2. Curb and gutter – bike lanes, sidewalk, multiuse path	\$ 6,980,000
Segment C	C1. Add bike lanes & sidewalk	\$ 3,100,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 900,000
	Total Alternative C1 Cost	\$ 4,020,000
	C2. Roadway reconstruction with curb and gutter – add bike lanes & sidewalk	\$ 6,500,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 900,000
	Total Alternative C2 Cost	\$ 7,420,000
	C3. No roadway improvements – add raised Boardwalk*	\$ 1,310,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 1,750,000
	Total Alternative C3 Cost	\$ 3,040,000
	C4. Bridge roadway – add sidewalk and paved shoulders	\$ 43,950,000
	Permitting	\$ 3,000
	Kayak Crossing	\$ 1,750,000
	Total Alternative C4 Cost	\$ 44,900,000
*Does not include \$72,000 yearly maintenance cost		

The lower preliminary cost alternative for Segment B is alternative B1 (approximately \$1,980,000) which widens the roadway with four-foot bike lanes, sidewalk on the north side of Walton Road and a shared use path on the south. This alternative would require a swale and is much lower cost than alternative B2 (\$6,980,000) which includes roadway reconstruction with curb and gutter.



For Segment C, Alternative C3 with the kayak crossing over Walton Road (\$3,040,000) has the lowest cost since no roadway improvements are proposed. The alternative would require an approximate annual maintenance cost of \$72,000, and the boardwalk would need to be replaced approximately every sixteen years.

An exhibit of these proposed recommendations as well as cost estimates are provided in **Appendix F**. ROW costs are not included. Wetland impacts are anticipated for all alternates presented in Segment C, therefore permitting will be required. Wetland impacts and mitigation costs are not included in the cost estimates. It is recommended that pre-application meetings be held with South Florida Water Management District (SFWMD) and the US Army Corps (land owners) in order to ensure that the alternative will avoid and minimize impacts.



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AGENDA ITEM SUMMARY

Board/Committee:	St. Lucie TPO Board
Meeting Date:	August 3, 2016
Item Number:	9e
Item Title:	Public Involvement Program (PIP) Annual Evaluation of Effectiveness and Update
Item Origination:	Unified Planning Work Program (UPWP)
UPWP Reference:	Task 6.1 – Public Involvement, Education & Outreach
Requested Action:	Accept the PIP Annual Evaluation of Effectiveness and Update, accept with conditions, or do not accept.
Staff Recommendation:	Based on the performance results and the Public Outreach Matrix demonstrating where the PIP is effective, it is recommended that the PIP Annual Evaluation of Effectiveness and Update be accepted by the TPO Board.

Attachments

- Staff Report
- PIP Performance Measures
- Public Outreach Matrix



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MEMORANDUM

TO: St. Lucie TPO Board

THROUGH: Peter Buchwald
Executive Director

FROM: Yi Ding
Livability Planner

DATE: July 15, 2016

SUBJECT: Public Involvement Program (PIP) Annual Evaluation
of Effectiveness and Update

BACKGROUND

The Unified Planning Work Program (UPWP) includes substantial public involvement efforts within a continuous and ongoing task which includes compliance with all Title VI and Nondiscrimination requirements. These public involvement efforts are advanced through the PIP of the St. Lucie TPO.

Annually reviewing the effectiveness of the PIP Goal, Objectives, and Strategies enables the TPO to adjust and modify the public involvement techniques accordingly. Various performance measures identified in the attached summary were used by TPO staff to evaluate the effectiveness of the PIP. The results of the annual Evaluation of Effectiveness can quantify public access to transportation planning activities as well as the level of public participation and impact within FY 2015-16. The public participation tools and techniques in the attached public outreach matrix subsequently are continued, refined, and/or replaced based on the results.

ANALYSIS

The PIP Annual Update includes evaluating the effectiveness of the PIP by applying the performance measures to the strategies for meeting the PIP objectives. It is the second time that performance measures data has been

collected, and the results will be used to compare to last year's data for evaluating and refining the PIP tools and techniques.

The three greatest measures for FY 2015/16 were the increase in the number of online hits to the TPO website and social media (13,188), the increase in the number of students who participated in TPO activities (5,315), and the increase in questionnaire recipients who responded to the questionnaire (1,885). These increases are attributed to the extensive social media outreach conducted for the first time as part of the public involvement efforts associated with the development of the Go2040 Long Range Transportation Plan and the FY 2016/17 – FY 2017/18 Unified Planning Work Program.

Revising the Public Outreach Matrix with the addition of new tools and techniques is part of the Annual Update as well. Based on the performance measures, "Social Media" has provided the best results, and it is recommended to extend the use of "Social Media" as part of the public involvement efforts associated with the upcoming Congestion Management Process (CMP) Major Update.

At their meetings in July, the TPO Technical Advisory Committee (TAC), Citizens Advisory Committee (CAC), and Bicycle-Pedestrian Advisory Committee (BPAC) recommended the acceptance of the PIP Annual Evaluation of Effectiveness and Update.

RECOMMENDATION

Based on the performance results and the Public Outreach Matrix demonstrating where the PIP is effective, it is recommended that the PIP Annual Evaluation of Effectiveness and Update be accepted by the TPO Board.

Public Involvement Plan - Performance Measures and Annual Update FY 15-16

Objective A - Hold Regularly Scheduled and Advertised Meetings Open to the General Public					
Strategy	Measure of Effectiveness				
	Measure	Quantity FY 14-15	Quantity FY 15-16	Number Change	Percent Change
Strategy A1: All TPO Policy Board and Advisory Committee meetings, times, agendas, and locations will be publicly noticed in local newspapers and online.	Overall increase in attendance at meetings	372	484	112	30%
Strategy A2: Video recorded TPO meetings will be available for replay on the local government channels and the internet (St. Lucie Online - Video on Demand - via link from TPO website - www.stlucietpo.org).	Increase in number of online hits to TPO website and social media	566	13,754	13,188	2330%
Strategy A3: TPO meetings will be held in ADA-compliant locations accessible to transit and traditionally underserved communities	Overall increase in number of meeting participants who are transit riders and/or the physically challenged.	4	19	15	375%
Strategy A4: TPO meetings and workshops will be publicly noticed in a variety of means to ensure that a variety of communities are reached and a scrapbook of the notifications will be maintained. Build data base of all meeting requests, concerns and inquiries regarding meeting schedules and logistics.	Overall increase in meeting attendance by minority and traditionally underserved communities	85	108	23	27%
Strategy A5: Ensure that all Advisory Committee positions are filled.	Overall decrease in the number of meetings where advisory committee positions remained unfilled.	0	0	0	N/A
Strategy A6: Ensure all Advisory Committee recommendations are presented to the TPO Board. Keep log of all Committee recommendations to the TPO Board and the respective actions taken by the Board.	Percentage increase in number of persons contacted for all surveys and questionnaires	23,584	7,451	-16,133	-68%
Strategy A7: Incorporate specialized surveys and questionnaires in meetings and events where appropriate.	Percentage increase in recipients who responded to the questionnaire	799	2,684	1,885	236%

Public Involvement Plan - Performance Measures and Annual Update FY 15-16

Objective B - Seek Out Traditionally Underserved Communities					
Strategy	Measure of Effectiveness				
	Measure	Quantity FY 14-15	Quantity FY 15-16	Number Change	Percent Change
Strategy B1: The TPO will create electronic newsletters and pamphlets describing its purpose and activities to be widely distributed throughout the TPO area as well as placed on the TPO website. Materials will also be made available in Spanish and Haitian Creole.	Percentage increase in meeting attendees who indicate they participated in the TPO outreach efforts	0	10	10	N/A
Strategy B2: All televised TPO meetings will be closed-captioned for the hearing impaired.					
Strategy B3: TPO notices, meeting agendas, and packets will be sent to minority communities or traditionally underserved populations to ensure notification of TPO events.	Overall increase in the participation of Spanish and Creole speakers.	6	8	2	33%
Strategy B4: TPO staff will provide presentations to community groups throughout the County to discuss the TPO process and projects.					
Strategy B5: Tailor outreach methods according to socio-economic characteristics summarized in the community profiles.	Percentage increase of traditionally underserved and minority community participants in TPO outreach efforts	0	34	34	N/A
Strategy B6: Identify communities with higher concentrations of minority and/or under-served populations, develop relationships with community and religious leaders in these communities, and hold workshops and meetings in these communities.	Overall increase in participation by religious and community leaders in minority and underserved communities.	30	35	5	17%
Strategy B7: A contact list of minority and underrepresented citizens will continue to be maintained and updated by the TPO.					
Strategy B8: Utilize public transportation equipment and infrastructure for advertising community-based TPO workshops and planning activities.	Overall increase in participants who use public transportation.	2	3	1	50%

Public Involvement Plan - Performance Measures and Annual Update FY 15-16

Objective C - Engage the Public Clearly, Continually, and Comprehensively through a Variety of Outreach Activities to Maximize Public Impact					
Strategy	Measure of Effectiveness				
	Measure	Quantity FY 14-15	Quantity FY 15-16	Number Change	Percent Change
Strategy C1: Schedule at least two workshops or community presentations per year in the evening and/or at locations other than government buildings to increase public awareness and outreach.	Overall increase in the percentage of telephone, email, and regular mail public inquiries to TPO programs and planning activities.	20	23	3	15%
Strategy C2: Track and keep records of public comments and contacts to the TPO through telephone calls, emails, letters, and include summary in annual update.					
Strategy C3: Use TPO website to improve amount of information available, its appeal, its usefulness, and make the site interactive so that public input can be obtained electronically.	Percentage increase in positive public responses to website performance and interaction capabilities.	1	4	3	300%
Strategy C4: Continue to explore and document new and/or alternative tools and techniques for reaching out to the community and facilitating public impact including advancements in social media outlets, engagement activities, and the TPO Visualization Campaign.	Overall increase in the percentage of comment forms and survey responses.	584	73	-511	-88%
Strategy C5: Create an outreach link with local schools and universities to emphasize the relationships between transportation, mobility, and livability.	Overall increase in the number of students who participate in TPO activities.	4,235	9,550	5,315	126%

Public Involvement Plan - Performance Measures and Annual Update FY 15-16

Objective D - Integrate the Principles and Special Projects Adopted in the 2040 LRTP in Public Outreach Efforts, Emphasizing Key Concepts of the Plan Including Enhanced Mobility, the US 1 Corridor Retrofit Project, and the Treasure Coast Loop Trail					
Strategy	Measure of Effectiveness				
	Measure	Quantity FY 14-15	Quantity FY 15-16	Number Change	Percent Change
Strategy D1: Partner with other transportation planning agencies, local governments, and community leaders to broaden the awareness of TPO priorities and objectives in the Go2040 LRTP, especially in traditionally under-served communities.	Overall increase in the number of non project-specific meetings with other planning agencies to discuss broader range issues.	6	9	3	50%
Strategy D2: Include key concepts of the Go2040 LRTP in outreach tools including TPO newsletters, annual reports, and project postcards.	Overall increase in the output of Go2040 LRTP special project visualization products (postcards, flyers, posters, etc).	N/A	3	N/A	N/A
	Overall increase in the number of Go2040 LRTP implementation projects underway.	N/A	16	N/A	N/A
Strategy D3: Maintain the Go2040 LRTP website and outreach database to ensure continuity through each future planning effort.	Percentage increase in the number of public inquiries regarding the Go2040 LRTP-specific planning efforts and special projects.	N/A	2	N/A	N/A

Public Outreach Matrix

		PIP	LRTP	TIP		UPWP	CMP	Projects
				Priorities	Document			
Public Review and Comment	Comment Forms	X	X	X	X		X	X
	Surveys & Questionnaires		X					
	Email, Mail, In Person, or Telephone Comments		X		X			
	Formal Review and Comment Periods	X	X		X	X		
TPO Website		X	X	X	X	X	X	X
Social Media Networking (Facebook/Twitter)		X	X	X	X	X	X	X
TPO Committee and Board Meetings		X	X	X	X	X	X	X
Visualization Campaign	TPO Newsletter							
	TPO Gallery and Kiosks		X					X
	Posters, Flyers, and Brochures		X					
Engagement Activities	Public Workshops or Charrettes		X					X
	Event Booths		X			X		
	Community Associations							
	Telephone Town Hall Meetings		X					
Efficient Transportation Decision-Making Process		N/A				N/A		
Media and Meeting Notification	Newspapers		X		X	X		
	Television							
	Radio							
	Direct Mailing							
	Email	X	X	X	X	X	X	X

PIP: Public Involvement Program LRTP: Long Range Transportation Plan TIP: Transportation Improvement Program

UPWP: Unified Planning Work Program CMP: Congestion Management Process N/A: Not applicable